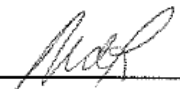


The Corporation of the Town of Ajax
COMMUNITY AFFAIRS AND PLANNING COMMITTEE



Monday, March 23, 2015 at 7:00 p.m.
Council Chambers, Town Hall
65 Harwood Avenue South

Confirmed by: 

AGENDA

Alternative formats available upon request by contacting:
sarah.moore@ajax.ca or 905-619-2529 ext. 3347

Anything in **blue** denotes an attachment/link. By clicking the links on the agenda page, you can jump directly to that section of the agenda. To manoeuvre back to the agenda page use the **Ctrl + Home** keys simultaneously. **OR** use the "Bookmark" icon to the left of your screen to navigate from one report to the next

C. Jordan, Chair

1. **Call To Order**
2. **Disclosure of Pecuniary Interest**
3. **Adoption of Minutes**
 - 3.1 **February 17, 2015**..... 2
4. **Public Meeting**
 - 4.1 **Salem Road Subdivision Ltd.**
Zoning By-law Amendment Z3/14
Draft Plan of Subdivision S-A-2013-01
Site Plan Application SP4/14
Northeast corner of Salem Road North and Harper-Hill Drive, P. Allore, Director of
Planning & Development Services / B. Scopacasa, Development Planner..... 5
5. **Presentations**

None
6. **Reports**

None
7. **Adjournment**

**Minutes of the
Community Affairs & Planning Committee Meeting
Held in the Council Chambers, Ajax Town Hall,
At 7:00 p.m. on February 17, 2015**

***Alternative formats available upon request by contacting:
sarah.moore@ajax.ca or 905-619-2529 ext. 3347***

Present:	Chair - Regional Councillor	- S. Collier
	Regional Councillor	- C. Jordan
	Councillors	- M. Crawford
		- R. Ashby
		- J. Dies
	Mayor	- S. Parish

Regrets:	Councillor	- P. Brown
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1. Call to Order

Chair Collier called the meeting to order at 7:00 p.m. It was noted that Councillor Brown had sent her regrets.

2. Disclosure of Pecuniary Interest

There were no disclosures of pecuniary interest.

3. Adoption of Minutes

Moved by: C. Jordan
Seconded by: R. Ashby

That the Minutes of the Community Affairs and Planning Committee Meeting held on June 16, 2014, be adopted.

CARRIED

4. Public Meetings

**4.1 Highway 401 from Brock Road to Courtice Road
Class Environmental Assessment and Preliminary Design
Town of Ajax Considerations**

Hubert Ng, Senior Transportation Planner introduced representatives from the Ministry of Transportation and their consultants. David Fallows, Ministry of Transportation made opening remarks and invited Patrick Puccini & Michael Weldon, consultants from AECOM, to deliver a presentation on the 401 from Brock Road to Courtice Road Study. An overview

was provided of the recommended long-term plan for enhancing this segment of the 401, and anticipated impacts in Ajax, specifically on properties that will be directly affected by the proposed lane expansions. The presenters noted that the ultimate Highway 401 improvements are not currently included in MTO's 5-year capital plan, and therefore it was not possible for them to place a timeline on when the detail design or construction may commence.

Committee members posed several questions to the presenters, primarily related to how MTO would acquire properties that are needed for the proposed works. It was explained that certain properties may indeed become inhabitable and their owners will be displaced. In other areas, there may be only a minor encroachment. The presenters explained that MTO would strive to acquire all required properties through negotiated settlement, with expropriation as a last resort. Persons in the affected areas who are currently trying to sell their homes and are unable to within 6 months due to forecasted 401-related MTO works can contact the MTO to discuss property-acquisition in the short-term. In response to further questions from the Committee, the following information was provided:

- The approximate timeframe between detail design and construction commencement is three years;
- MTO is not recommending the introduction of an interchange at Church St; any party wishing to propose an interchange at Church St would have to demonstrate to MTO that it would not impact traffic flow on the highway;
- In any instances where parking space is being lost due to 401 works, MTO would be required to either compensate for the lost parking elsewhere on the site, or obtain the proper variances for reduced parking.

Chair Collier invited comments from members of the public in attendance.

Sergio Munoz, 89 Mill St, sought information on how his particular property would be affected by the proposed works, noting that his property has a very low elevation and would therefore require a high sound barrier which may impact the amount of sunlight that reaches his property. Further questions included when construction would commence and whether property owners would receive fair market value for their homes.

William Davenport, 3 Tudor Ave, requested further information about the proposed works affecting Tudor Avenue. He noted that he had recently invested in significant interior renovations in his home and wondered whether this would be a factor in price negotiations with MTO.

The presenters responded to questions from residents, noting that they would follow up individually with property owners regarding their particular circumstances.

There being no further comments, the Committee moved on to consideration of the staff recommendation.

Moved by: J. Dies
Seconded by: M. Crawford

That the report to Community Affairs and Planning Committee entitled "Highway 401 from Brock Road to Courtice Road – Class Environmental Assessment and Preliminary Design – Town of Ajax Considerations" dated February 17, 2015, be received for information.

That the Ministry of Transportation be advised that the Town of Ajax does not support the introduction of an interchange at Highway 401 and Church Street.

CARRIED

5. Presentations

6. Reports

None.

7. Adjournment (7:50 p.m.)

Moved by: M. Crawford
Seconded by: C. Jordan

That the February 17, 2015 meeting of the Community Affairs and Planning Committee be adjourned.

CARRIED

N. Wellsbury , D-Clerk

TOWN OF AJAX REPORT



REPORT TO: Community Affairs and Planning Committee

SUBMITTED BY: Paul Allore, MCIP, RPP
Director of Planning and Development Services

PREPARED BY: Bruno Scopacasa, MES, MCIP, RPP
Senior Development Planner

SUBJECT: Salem Road Subdivision Ltd.
Zoning By-law Amendment Z3/14
Draft Plan of Subdivision S-A-2013-01
Site Plan Application SP4/14
Northeast corner of Salem Road North and Harper-Hill Drive

WARD: 2

DATE OF MEETING: March 23, 2015

REFERENCE: Draft Plan of Subdivision S-A-2005-01

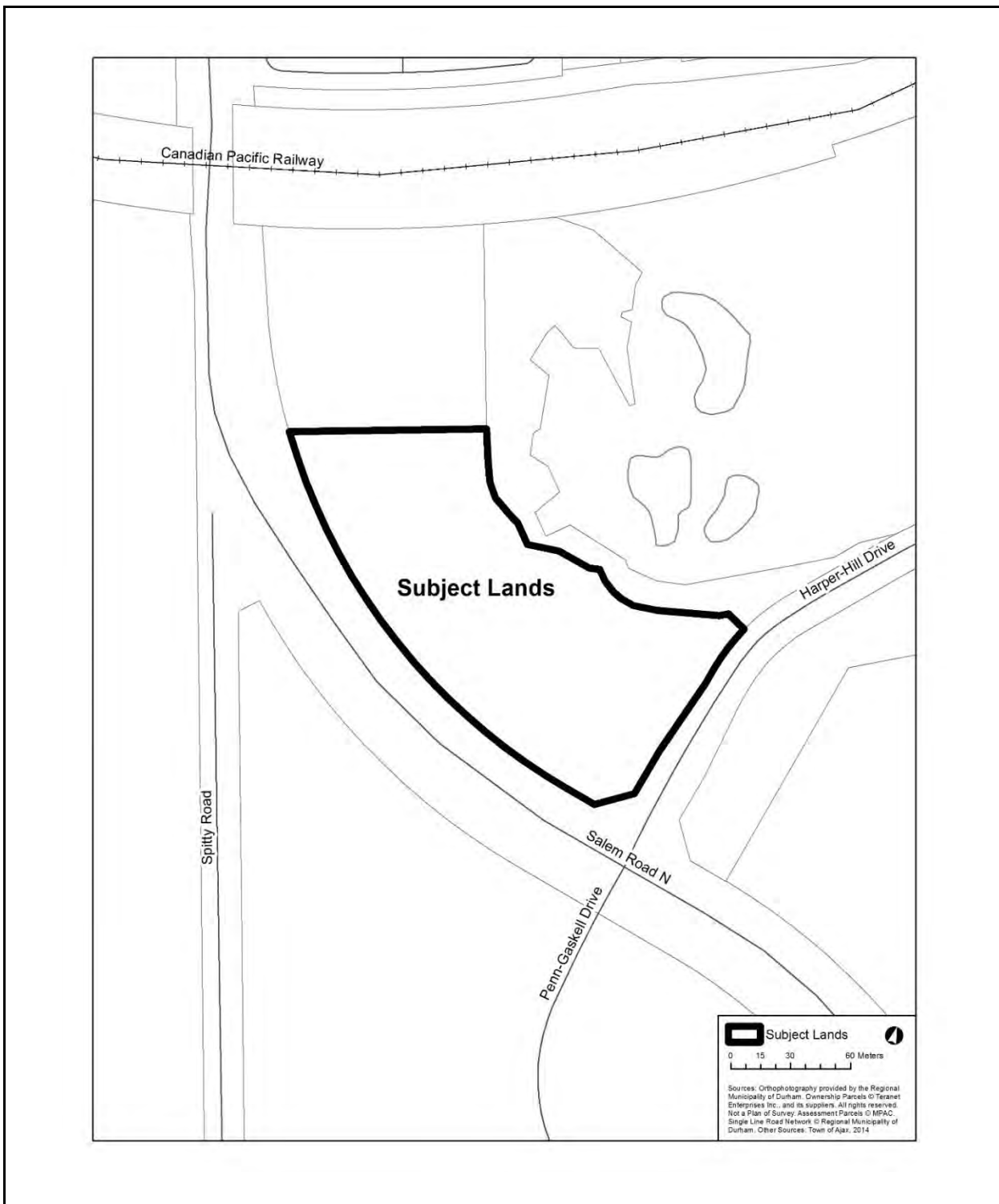
RECOMMENDATIONS:

1. That Zoning By-law Amendment Z3/14, submitted by Salem Road Subdivision Ltd., be approved and that staff be authorized to prepare and forward an implementing zoning by-law to Council for its consideration at a future meeting, as provided within Attachment 1 to this report;
2. That Draft Plan of Subdivision S-A-2013-01, submitted by Salem Road Subdivision Ltd., be endorsed and that staff be authorized to grant draft approval of the plan of subdivision, subject to the proposed draft conditions, as provided within Attachment 2 to this report; and
3. That Site Plan SP4/14, submitted by Salem Road Subdivision Ltd., be endorsed, and that staff be authorized to grant final site plan approval subject to finalizing all required drawings to the satisfaction of the Town of Ajax.

1.0 BACKGROUND:

In March 2014, Salem Road Subdivision Ltd. (Stafford Homes), submitted development applications proposing to develop the subject lands located at the northeast corner of Salem Road North and Harper-Hill Drive (see Figure 1).

The purpose of this report is to discuss and recommend approval of a zoning by-law amendment, draft plan of subdivision and a site plan to develop 92, 3-storey multiple attached dwellings within a common element condominium.



Application File Nos.:
Z3/14, S-A-2013-01 & SP4/14

Applicant:
Salem Road Subdivision Ltd.

Date: March 23, 2015

Figure 1
Subject Lands
Location Map

Town of Ajax
Planning and
Development
Services



The subject lands are a medium density residential block within the former Runnymede Beechridge subdivision S-A-2005-01 which was approved by the Ontario Municipal Board in July 2011.

1.1 Property Location and Surrounding Land Use Context

The subject lands encompass an area of 2.02 hectares (4.99 acres) with 248 metres (814 feet) of frontage onto Salem Road North and 99 metres (325 feet) of frontage onto Harper-Hill Drive. The subject lands are currently vacant and are surrounded by the following land uses.

North: To the north is a Rogers Communications facility. Further north is the Canadian Pacific Railway and a residential neighbourhood consisting of townhouses and single detached dwellings within the Mountcliffe residential subdivision.

East: To the east are Town owned lands designated Environmental Protection which contain a wetland and a gravel nature trail. Further east are single detached dwellings within the Monarch Tranquility residential subdivision.

South: To the south is Harper-Hill Drive and vacant lands designated Prestige Employment. Further south are prestige employment lands within the Perlane development that contain a medical building, restaurants and various other service oriented uses.

West: To the west is Salem Road North. Further west are vacant lands designated Prestige Employment, Spitty Road and the dual high school campus of Notre Dame Catholic Secondary School and J. Clarke Richardson Collegiate.

1.2 Proposal

Stafford Homes is proposing to develop 92, 3-storey multiple attached dwellings, a private parkette, private roads and visitor parking in a common element condominium format. A total of 40 units are proposed along Salem Road North and Harper-Hill Drive with the remaining 52 units internal to the site. Dwelling units will range from 1,176 ft² to 2,130 ft² in size. One full movement vehicular access is proposed from Harper-Hill Drive that leads to two internal private roads. Secondary emergency access is proposed from Salem Road North (see Figure 2).

The proposal provides for a mid-block pedestrian connection from Salem Road North through the site to a private parkette, which connects to a branch of the Carruthers Creek Trail. The parkette will be equipped with a playground, seating, shade structure and a walkway that will connect to the gravel nature trail located on Town owned lands to the east. Each unit is designed with a minimum of two parking spaces, one within a private garage and one within a driveway. Eleven units are designed to have a double car garage. Visitor parking is provided and has been located throughout the site.

The subject lands are currently zoned to permit townhouse dwellings. The submitted zoning by-law amendment proposes development standards that would implement the proposed development and make the Parcels of Tied Land (POTLs) legally conveyable lots. The submitted draft plan of subdivision serves to create a block where the proposed 92 POTLs will be created through the part lot control exemption process.

The development statistics are summarized in Table 1.



Application File Nos.:
Z3/14, S-A-2013-01 & SP4/14

Applicant:
Salem Road Subdivision Ltd.

Date: March 23, 2015

Figure 2
Proposed Site Plan

Town of Ajax
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Development
Services



Table 1: Summary of Proposed Development

Proposed Land Use	No.	Area hectare (acre)	Area Percentage (%)
Townhouse Units	92	1.49 ha (3.68 ac)	73.7 %
Visitor/Overflow Parking	24	0.04 ha (0.10 ac)	2.0 %
Parkette	-	0.07 ha (0.18 ac)	3.6 %
Private Roads and Sidewalks	-	0.38 ha (0.95 ac)	19.0 %
Other	-	0.03 ha (0.08 ac)	1.6 %
Total	-	2.01 ha (4.99 ac)	100%

2.0 DISCUSSION:

2.1 Provincial Policy Statement

The Provincial Policy Statement (PPS) provides an overall direction on matters of provincial interest that must be reflected in municipal planning decisions. The PPS was issued under Section 3 of the *Planning Act* and provides provincial direction in terms of land use planning and development in Ontario. The current PPS document came into effect on April 30, 2014. The PPS states that healthy, livable, and safe communities are to be sustained, including among other matters, promoting efficient development and land use patterns, by accommodating an appropriate range and mix of residential, employment, recreational, and open space uses to meet long-term needs. Appropriate densities should be promoted for new housing which efficiently uses land resources, infrastructure, and public services. The PPS promotes all forms of housing required to meet social, health, and well-being requirements of current and future residents.

The proposed development is consistent with the policies of the PPS.

2.2 Growth Plan for the Greater Golden Horseshoe

The Growth Plan for the Greater Golden Horseshoe (Growth Plan) was approved under the authority of the *Places to Grow Act, 2005* by the Lieutenant Governor in Council of the Province of Ontario, and came into full force and effect on June 16, 2006. The Growth Plan provides a framework for managing growth in the Greater Golden Horseshoe including direction of where and how to grow, the provision of infrastructure to support growth, and protecting natural systems and cultivating a culture of conservation.

Under the Growth Plan, new development in designated greenfield areas will be planned, designated, zoned and designed in a manner that contributes to creating complete communities, creates street configurations, densities, and an urban form that support walking, cycling, and the early integration and sustained viability of transit services, provides a diverse mix of land uses to support vibrant neighbourhoods and creates high quality public open spaces with site and urban design standards that support opportunities for transit, walking and cycling.

The proposals conform to the provisions of the Growth Plan.

2.3 Region of Durham Official Plan

The subject lands are currently designated "Living Areas" within the Durham Regional Official Plan. Living Areas shall be used predominately for housing purposes and shall incorporate the widest mix of housing types, sizes, and tenures to meet the diverse housing needs of the residents of Durham Region.

The subject property abuts Salem Road North, which is designated as a “Regional Corridor” within the Durham Regional Official Plan. Regional Corridors are to be planned and developed as higher density mixed use areas that promote public transit ridership and provide urban design that orients development to arterial corridors.

The proposed development is consistent with the policies of the Region of Durham Official Plan.

2.4 Town of Ajax Official Plan

The Town of Ajax Official Plan designates the subject lands “Medium Density Residential”. Permitted uses within the Medium Density Residential designation include duplex dwellings, block townhouses, street townhouses, stacked townhouses, attached housing, walk-up apartments and other forms of moderately scaled housing within a density range of 30 to 75 units per net hectare. The development proposes a density of 45.4 units per net hectare.

The built environment policies of the Town of Ajax Official Plan require development to be planned to:

- promote street-oriented development, with active building facades fronting onto streets;
- promote built form where a significant portion of the street frontage is defined by the building and where parking is located on the inside of building mass;
- provide a design layout and form that is compatible adjacent existing and approved land uses;
- promote pedestrian and cyclist permeability and interconnectivity to abutting paths, trails, streets and development;
- locate buildings such that pedestrian spaces are well defined and connected;
- be compatible with adjacent existing in terms of height, density, scale, massing and character;
- maximize transparent windows facing the public realm;
- position primary entrances to be visible and accessible from the street; and
- define the transition from public to semi-public to private space through landscaping.

The proposed development has been designed in conformity with the built environment policies of the Town's Official Plan by providing for a high level of building design, interest and an enhanced streetscape along Salem Road North and Harper-Hill Drive. The layout of the development applies principles of safe community design by orienting residential dwellings towards public and private spaces. The proposed development is appropriately integrated with adjacent public spaces and is buffered from employment lands to the south and west by Harper-Hill Drive and Salem Road North.

The proposed development is consistent with the policies of the Town of Ajax Official Plan.

2.5 Town of Ajax Zoning By-law 95-2003

The subject lands are currently zoned Residential Multiple Five Zone with a Holding ‘H’ Provision [RM5 (H)] and are subject to Exception 163. The RM5 zone category permits multiple attached dwellings. Exception 163 provides setback requirements for the RM5 zone designed to establish a development envelope.

The conditions for the removal of the Holding ‘H’ Provision are:

1. the applicant submit a site plan application to the Town for approval; and,
2. that the applicant enter into a site/development agreement with the Town.

In order to implement the proposed development the applicant is seeking amendments to the site specific zoning by-law to establish detailed development standards, including provisions for frontage onto a private road, setbacks to dwellings, coverage and accessory buildings. The proposed amendment to Town of Ajax Zoning By-law 95-2003, as amended is provided in Attachment 1 to this report.

2.6 Development Design

The following sections will provide details of the key elements of the proposal including; site design, built form, landscaping, parking and vehicular circulation.

a) Site Plan

The proposed 92, 3-storey townhouse dwellings will be grouped within 16 blocks that will contain 4 to 7 units per block. The townhouse blocks are located along the perimeter of the site with an interior row of townhouse blocks and private roads (Graywardine Lane and Dolphin Lane) providing access. These townhouse blocks will consist of three types of units. Double fronted units are located along the sites Salem Road North and Harper-Hill Drive frontages, lane based units are located in the interior blocks and conventional front and rear yard units are located along the north and east limits of the site (see Figure 3).

Townhouse blocks have been oriented towards the sites public street fronts and the intersection of Salem Road North and Harper-Hill Drive to establish a strong building presence at the intersection, a more desirable streetscape and enhanced pedestrian experience.

Prior to the issuance of a building permit the applicant will be required to provide a construction management plan, which addresses among other matters, site access, construction traffic, parking for construction trades, staging, and mud, dust and noise controls, to the satisfaction of the Town of Ajax. This is reflected in the conditions of draft approval provided in Attachment 2 to this report.

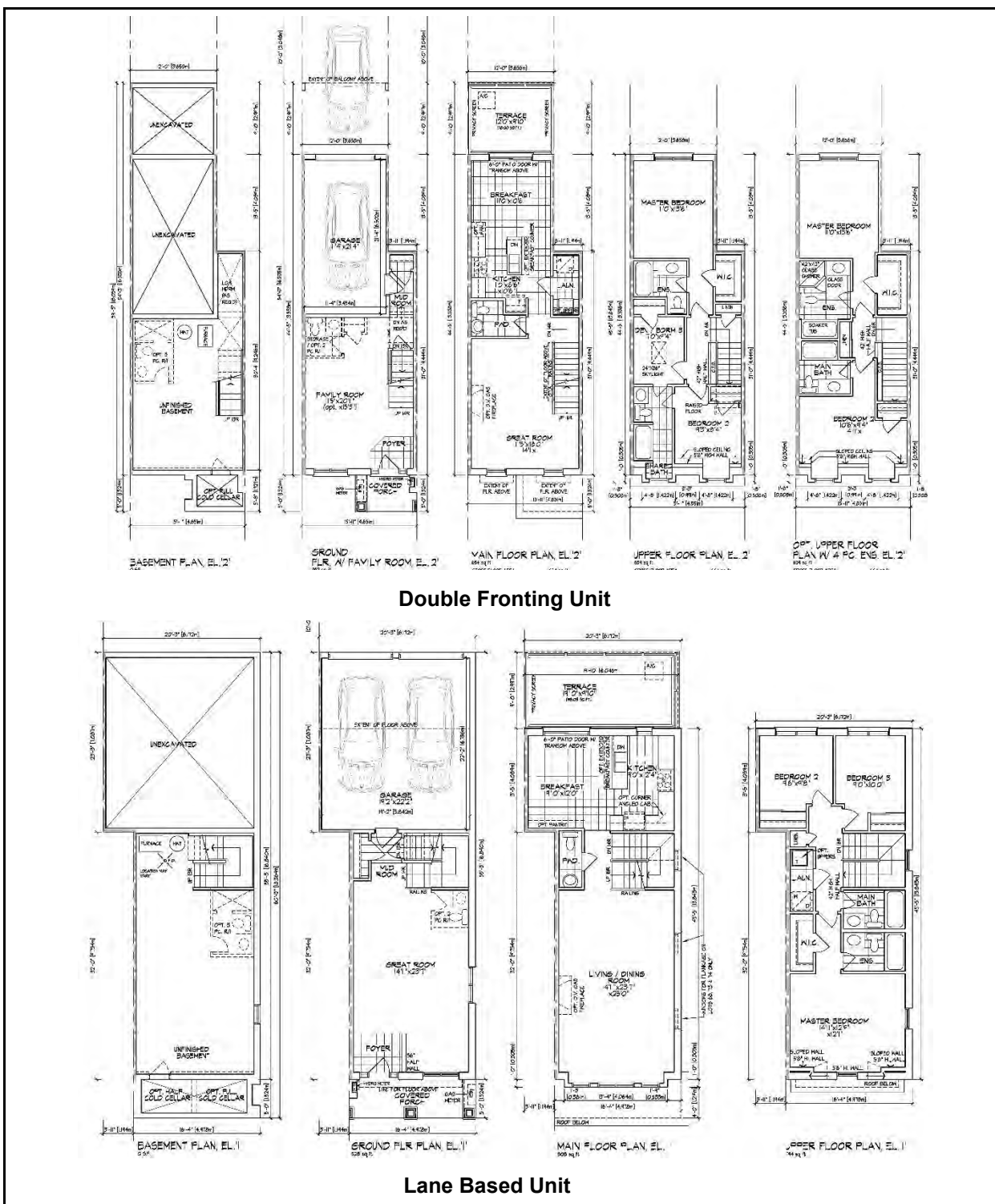
b) Parking

Each unit is designed with a minimum of two parking spaces. Eighty-one units are designed to have a single car garage and an additional parking space on a driveway. Eleven units are designed to have a double car garage. A total of 24 additional surface parking spaces are proposed and have been grouped in areas at the north and south ends of the site. Garages have been designed with overhead space to accommodate for mezzanine storage.

The proposed parking supply exceeds the Town's zoning by-law requirements as noted in Table 2 below.

Table 2: Summary of Required and Proposed Parking

Parking Type	Minimum Required Parking Spaces under Zoning By-law 95-2003	Proposed Parking Spaces
Residential	138 (1.5 parking spaces/unit)	184 (2 parking spaces/unit)
Additional Parking (Visitor/Overflow)	23 (0.25 parking spaces/unit)	24 (0.26 parking spaces/unit)



Application File Nos.:
Z3/14, S-A-2013-01 & SP4/14

Applicant:
Salem Road Subdivision Ltd.

Date: March 23, 2015

Figure 3
Proposed Floor Plans

Town of Ajax
Planning and
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c) *Elevations*

The proposed dwellings provide a high level of building design and treatment, while creating a strong building presence along Salem Road, Harper-Hill Drive and the internal private roads. Each townhouse block will be 3-storeys in height and feature covered porches, dormers, keystones and relief. Units are proposed to be clad in a stone base and brick veneer, and be equipped with a front porch, balcony or deck, or a combination thereof, ranging from 40 to 195 square feet in size. Primary entrances will be visible and accessible from the street and windows have been located to overlook the public realm.

All gas and hydro metres will be inset into the wall adjacent to the porch or garage door screening them from street view. A water meter room will be provided as a separate building located at the north end of the site and will be designed to blend with the architecture of the proposed dwelling units. Community mailboxes will be located in conjunction with the water metre room.

Overall, the proposed development will provide a high level of architectural design and will provide for a positive urban design with a strong streetscape along Salem Road North (see Figure 4).

d) *Vehicular and Pedestrian Access*

Vehicular access is proposed from Harper-Hill Drive. Two internal private roads, Graywardine Lane and Dolphin Lane, will provide access through the proposed development. Graywardine Lane will double as the required fire route and have secondary emergency access onto Salem Road North at the north limit of the site.

An internal pedestrian sidewalk is proposed along Graywardine Lane that will provide north-south pedestrian access through the site and connect to the existing sidewalk on Harper-Hill Drive and the future multi-use trail along Salem Road North. A mid-block pedestrian connection is proposed from the future Salem Road North multi-use trail east through the site to a private parkette. The pedestrian connection leads to the existing Carruthers Creek Trail to the east of the site. Street and pedestrian lighting is proposed for internal private roads and pedestrian walkways.

The Salem Road North multi-use trail, north of Rossland Road to the CP Railway, is scheduled to be constructed in 2015. The Town has requested that the multi-use trail across the frontage of the site be constructed by the applicant in order to harmonize the trail works with grading, sodding and street tree planting required by the applicant on the Salem Road North boulevard and with grading and landscape works located in front yards of dwelling units fronting the trail.

The construction of the multi-use trail across the frontage of the site will be coordinated with the construction of the trail to the north and south of the subject lands being undertaken by the Town. The applicant has agreed to enter into a Construction Administration Agreement with the Town to construct the Salem Road North multi-use trail across the frontage of the site in conjunction with the proposed development.

This is reflected in the conditions of draft approval provided in Attachment 2 to this report.



**Example Elevation Fronting
Salem Road North (Block 2)**



**Elevation Fronting Salem Road
North/Harper-Hill Drive Corner (Block 6)**



**Elevation Fronting Harper-Hill Drive
(Block 7)**

Application File Nos.:
Z3/14, S-A-2013-01 & SP4/14

Applicant:
Salem Road Subdivision Ltd.

Date: March 23, 2015

Figure 4
Proposed
Streetscape/Elevations

Town of Ajax
Planning and
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Services



e) *Landscape Design*

The applicant has proposed a combination of deciduous, coniferous and shrub plantings throughout the development. A 1.2 metre high decorative black metal fence and low lying landscaping is proposed along the Salem Road North and Harper-Hill Drive frontages. The combination of landscaping and fencing will create a strong pedestrian friendly urban edge and define the property limits. Fencing is proposed along the rear lot lines of Blocks 11 through to Block 16. This fencing serves as a noise attenuation barrier for Blocks 11 to 13 and for controlled access to Town owned lands to the east for Blocks 14 to 16.

A private parkette is proposed along the east limit of the site. The parkette will be equipped with a playground, seating, shade structure, bicycle racks and a walkway that will connect to the existing Carruthers Creek Trail located on Town owned lands to the east.

Detailed terraced landscaping is proposed within the daylighting triangle at the intersection of Salem Road North and Harper-Hill Drive. This landscaping is integrated with masonry piers, decorative metal fencing and unit paved walkways associated with Block 6 to create a strong corner expression in conjunction with the intersection oriented front elevations.

Wetland and forest buffer planting will be installed on adjacent lands to the east. The Toronto and Region Conservation Authority (TRCA) has reviewed and approved the proposed wetland and forest buffer planting.

3.0 REPORTS AND STUDIES

Below is a summary of the reports and studies filed in support of the application.

3.1 Functional Servicing Report

A Functional Servicing Report, prepared by GHD, has been submitted in support of the development proposals. The report indicates sanitary discharge from the development will outfall to the existing sanitary sewer located on Harper-Hill Drive. Watermain distribution will consist of a watermain and firemain located within the development road network and will connect to an existing 300 mm watermain on Salem Road North. Water quantity, quality and erosion control measures are provided by an existing stormwater management facility located north of Rossland Road East and west of the Carruthers Creek.

Engineering staff and the Toronto and Region Conservation Authority (TRCA) have reviewed the functional servicing report and are satisfied with the overall servicing approach.

3.2 Archaeological Assessment

In 2010, an Archaeological Assessment was prepared by Golder Associates in accordance with the *Ontario Heritage Act*. The assessment investigated the presence of archaeological resources on the subject site. Historical background research and field investigations were performed. The assessment concluded that the subject site is clear of any archaeological concerns.

3.3 Environmental Impact Study

Bird and Hale, Burnside & Associates Limited, and Sabourin & Kimble Associates Ltd. have prepared an Environmental Impact Study in support of the development proposal. The study concludes the development proposal will not result in a negative impact to area natural heritage features or environmental functions.

The development proposal retains and buffers all significant natural heritage features. Wetland and forest buffer planting are required on adjacent lands to the east. The Toronto and Region Conservation Authority (TRCA) has reviewed and approved the proposed wetland and forest buffer planting.

The buffer planting is reflected on the proposed landscape plans and will be secured through the required development agreement.

3.4 Phase I Environmental Site Assessment

A Phase I Environmental Site Assessment (ESA), prepared by Golder Associates, was prepared in support of the development proposal. Based on the review of historical records and findings from field observations, the Phase I ESA revealed that the subject site was utilized as agricultural lands since 1927. There was no visual evidence of staining, spills or stressed vegetation observed on-site. There was no evidence of aboveground storage tanks (AST) or underground storage tanks (UST) observed on site.

The report concludes that there are no issues of potential environmental concern pertaining to the subject lands.

3.5 Noise and Vibration Impact Study

A Noise and Vibration Impact Study, prepared by J.E. Coulter Associates Limited, was submitted in support of the development proposal. The study concludes that the noise levels generated from Salem Road North and the Canadian Pacific Railway (CP Rail) are expected to generate a noise impact that will require noise control measures to satisfy the municipality and Ministry of Environment (MOE) requirements.

The study recommends that a noise barrier be installed along the flankage and rear yards of Block 11 and rear yards of Block 12 to control outdoor living area sound levels. In order to meet MOE interior sound level criteria upgraded windows and brick veneer are required for Blocks 1, 11, 12 and 13. Air conditioning is required for all units in Blocks 1 to 6 and 11 to 13. Agreements of purchase and sale for these blocks will incorporate warning clauses notifying potential homeowners of the required noise control measures.

All purchase and sale agreements are required to include a warning clause notifying potential homeowners of the existence of the CP Rail and the possibility that the rail facility may be altered or expanded, which expansion or alteration may affect the living environment of residents despite the inclusion of noise attenuating measures. Warning clauses cannot legally be registered on title. In order to advise subsequent homeowners of the potential impacts of an expansion or alteration to the CP Rail facility, the warning clause is required to be included in all condominium documents, including the condominium declaration. This is reflected in the conditions of draft approval provided in Attachment 2 to this report.

3.6 Traffic Impact Study

A Traffic Impact Study, prepared by WSP, was submitted in support of the development proposal.

The study indicates that the proposed development will generate 50 AM (8 inbound and 42 outbound) and 58 PM (37 inbound and 21 outbound) peak hour trips. The capacity of the existing stop control T-intersection at Salem Road North and Harper-Hill Drive was reviewed based on site traffic generated from the proposed development and the existing Monarch Tranquility residential subdivision located to the east. The analysis concludes that the existing intersection will operate at an acceptable level of service.

Westbound left-turn movement from Harper-Hill Drive onto Salem Road North will have some delay in both the AM and PM peak hours due to heavy north/south traffic volumes on Salem Road North. This level of delay is typical in urban conditions for vehicles turning onto a major road with high traffic volume from a minor road under stop control conditions. The study concludes that traffic generated by the proposed development can be accommodated by the existing roadway network without any adverse impact.

The Salem Road North and Harper-Hill intersection will be signalized once warranted. This is anticipated to occur once vacant prestige employment lands to the south and west are developed. The signalization of the intersection has been secured through the Monarch Tranquility subdivision agreement.

4.0 FINANCIAL IMPLICATIONS:

There are no financial implications associated with the approval of these applications.

5.0 COMMUNICATION ISSUES:

5.1 Departments and Agency Comments

The applications have been circulated to all internal departments and external agencies and their comments and recommendations have been incorporated into this report.

5.2 Public Open House

A Public Open House was held on January 27, 2015. Notice of a Public Open House was circulated to all assessed persons within 120 metres (400 feet) of the subject lands and was also posted on the Town's Public Meetings web page and in the January 14 and 21, 2015 editions of the *Ajax News Advertiser*. No members of the public attended the meeting.

On March 2, 2015, notice of the Statutory Public Meeting was circulated to all assessed persons within 120 metres of the subject lands in accordance with the *Planning Act* requirements and was posted on the Town's Public Meetings web page. In addition, notification of the Statutory Public Meeting was placed in the March 11 and 18, 2015 editions of the *Ajax News Advertiser*. At the time of submission of this report to the Legislative and Information Services Department, no additional written correspondence from the public had been received.

6.0 CONCLUSION:

Staff have reviewed the submitted applications and are of the opinion that the proposed development will provide a compact urban built form along a significant corridor within the Town of Ajax. The overall design and layout will be in keeping with the character of the surrounding area. The development is consistent with the PPS, Growth Plan, Region of Durham and Town of Ajax Official Plan policies.

Staff recommend approval of the proposed implementing zoning by-law, draft plan and site plan subject to the recommendations provided in this report.

Bruno Scopacasa, MES, MCIP, RPP
Senior Development Planner

Gary Muller, MCIP, RPP
Manager of Planning

Paul Allore, MCIP, RPP
Director, Planning and Development
Services

ATT-1 - Proposed Zoning By-law Amendment
ATT-2 - Conditions of Draft Approval

ATT-1 - Proposed Zoning By-law Amendment

THE CORPORATION OF THE TOWN OF AJAX

BY-LAW NUMBER XX-2015

A By-law to amend By-law No. 95-2003, as amended.

WHEREAS, authority is granted under Section 34 of the *Planning Act*, R.S.O., 1990, c. P.13, for Council to pass this By-law;

AND WHEREAS, the Council of the Corporation of the Town of Ajax has approved an application to amend By-law No. 95-2003, as amended (Z3/14), with respect to the subject lands described as PART OF LOT 5, CONCESSION 3, NOW PART OF PARTS 1 AND 3, PARTS 2, 8 AND 9 ON RP 40R-26814, identified on Schedule 'A' to this By-law;

AND WHEREAS, the Council of the Town of Ajax deems it appropriate to pass an implementing Zoning By-law to regulate the future development of the subject lands;

NOW THEREFORE, the Council of the Corporation of the Town of Ajax enacts as follows:

1. THAT the Zone Schedule, specifically Map 18, attached to and forming part of Zoning By-law No. 95-2003, as amended, is hereby further amended by rezoning a portion of the subject lands from "Residential Multiple Five, Holding '(H)' Provision, with Exception 163 [RM5 (H), Exception 163]" to "Private Open Space, Holding '(H)' Provision, with Exception 163 [POS (H), Exception 163]" as indicated on the attached Schedule 'B' of this By-law.
2. THAT Exception 163, under Section 7.1.1, List of Exceptions, of By-law No. 95-2003, as amended, is hereby further amended as follows:
 - a) By adding By-law No. XX-2015 in the "By-law" reference header, such that it appears after and in conjunction with "73-2011";
 - b) By adding "S-A-2013-01", "SP4/14" and "Z3/14" under the "File Reference" reference header such that it appears after and in conjunction with "Z1/05, OPA05-A1, S-A-2005-01, OMB Approved";
 - c) By deleting the text "and RM5 – Residential Multiple Five Zone" associated with bullet "iii) Interpretation:";
 - d) By adding bullet "a)" after "iii) Interpretation:" such that it appears in conjunction with the subsequent text and below "iii) Interpretation:";
 - e) By adding the following new bullets such that they appear after bullet "iii) Interpretation a)" as follows:

- “b) Parcels of tied land zoned Residential Multiple Five Zone (RM5) for *Multiple Attached, Dwelling* in a common element condominium bordered by Salem Road North and Dolphin Lane or Harper-Hill Drive and Dolphin Lane or Dolphin Lane and Graywardine Lane are deemed to be lots with two *Front Yards*.”
- “c) Section 4.9 of Zoning By-law 95-2003, as amended, shall not apply to lands zoned Residential Multiple Five Zone (RM5) for *Multiple Attached, Dwelling* in a common element condominium.”
- f) By deleting the text associated with bullets “a)”, “b)” and “c)” under bullet “v) Development Standards for the ‘RM5’ Zone.” in its entirety and substituting therefore the following:

Development Standards:

- a) Minimum *Lot Frontage*: 4.8 m
- b) Minimum *Lot Depth*: 22.0 m
- c) Minimum Setback from *Front Lot Line* for *Dwelling Unit* located within Blocks referenced below and illustrated in Schedule ‘C’:
 - i) Blocks 1 to 6 (inclusive) fronting onto Salem Road North: 3.0 m
 - ii) Blocks 1 to 7 (inclusive) fronting onto Dolphin Lane: 3.7 m
 - iii) Blocks 6 and 7 fronting onto Harper-Hill Drive: 3.0 m
 - iv) Blocks 8 to 10 (inclusive) fronting onto Dolphin Lane: 2.4 m
 - vi) Blocks 8 to 10 (inclusive) fronting onto Graywardine Lane: 2.2 m
 - vii) Blocks 11 to 16 (inclusive) fronting onto Graywardine Lane: 2.5 m
- d) Maximum Setback from *Front Lot Line* for *Dwelling Unit* located within Blocks referenced below and illustrated in Schedule ‘C’:
 - i) Blocks 1 to 6 (inclusive) fronting onto Salem Road North: 6.2 m
 - ii) Blocks 6 and 7 fronting onto Harper-Hill Drive: 4.3 m
- e) Minimum Setback from *Interior Lot Line*
 - i) Between all horizontally attached dwellings units: 0.0 m
 - ii) For all end units: 1.2 m
- f) Minimum Setback from *Exterior Lot Line*: 1.0 m
- g) Minimum Setback from *Rear Lot Line*: 4.7 m
- h) Maximum *Lot Coverage* for Parcels of Tied Land (POTL) associated with Blocks referenced below and illustrated in Schedule ‘C’:
 - i) Blocks 1 to 6 (inclusive) 55%
 - ii) Blocks 7 to 10 (inclusive) 78%
 - iii) Blocks 11 70%
 - iv) Blocks 12 to 16 (inclusive) 60%

- i) Maximum *Height*: 12.5 m
- j) Maximum Garage Width: 6.2 m
- k) Other Development Standards:
 - i) Minimum separation between each *Multiple Attached Dwelling* block: 2.4 m
 - ii) *Dwelling Units* with a single vehicle *Private Garage*, must maintain a minimum *Driveway Width* of 2.7 metres and minimum *Driveway* depth of 6.0 metres.
 - iii) The maximum *Driveway Width* shall not exceed the exterior dimensions of the *private garage*.
 - iv) *Accessory buildings* or *structures* shall not be permitted in a *Side Yard*.
 - v) Notwithstanding the Minimum Setback from the *Front Lot Line* and *Exterior Side Lot Line* under Section 4.3 (Platforms) the following shall apply:
 - *Platforms* may extend 3.0 metres out from main building wall, but not closer to a *front lot line* or *exterior lot line* then 1.0 metre
 - All other provisions under Section 4.3 (Platforms) shall apply.
 - vi) A 1.2 metre height decorative metal fence shall be located in *Front Yards* adjacent to Salem Road North and Harper-Hill Drive. The decorative metal fence shall be setback 2.0 metres from the *Front Lot Line* abutting Salem Road North and Harper-Hill Drive.
 - vii) Only fencing approved pursuant to SP4/14 is permitted.
 - viii) Lands zoned Private Open Space (POS) shall be permitted to erect *buildings* or *structures* and alter soil or topography in order to construct a private parkette pursuant to SP4/14.

Other:

- a) Only 92 *Multiple Attached Dwelling* Units are permitted on Part 1, Plan 40R-27607

3. By-law No. 95-2003, as amended, is hereby further amended only to the extent necessary to give effect to the provisions of this By-law.

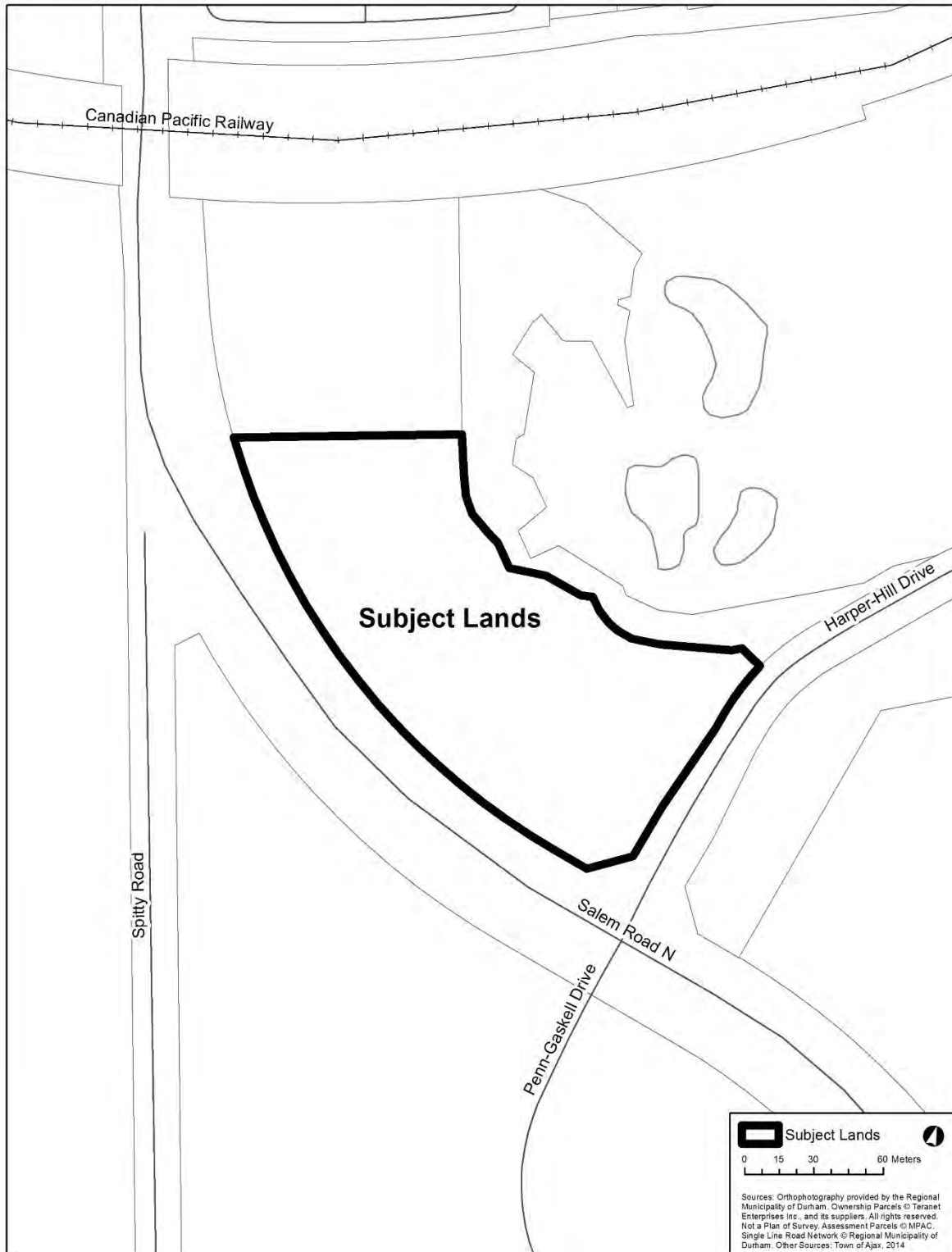
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THIRTIETH day of March, 2015

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THIRTIETH day of March, 2015

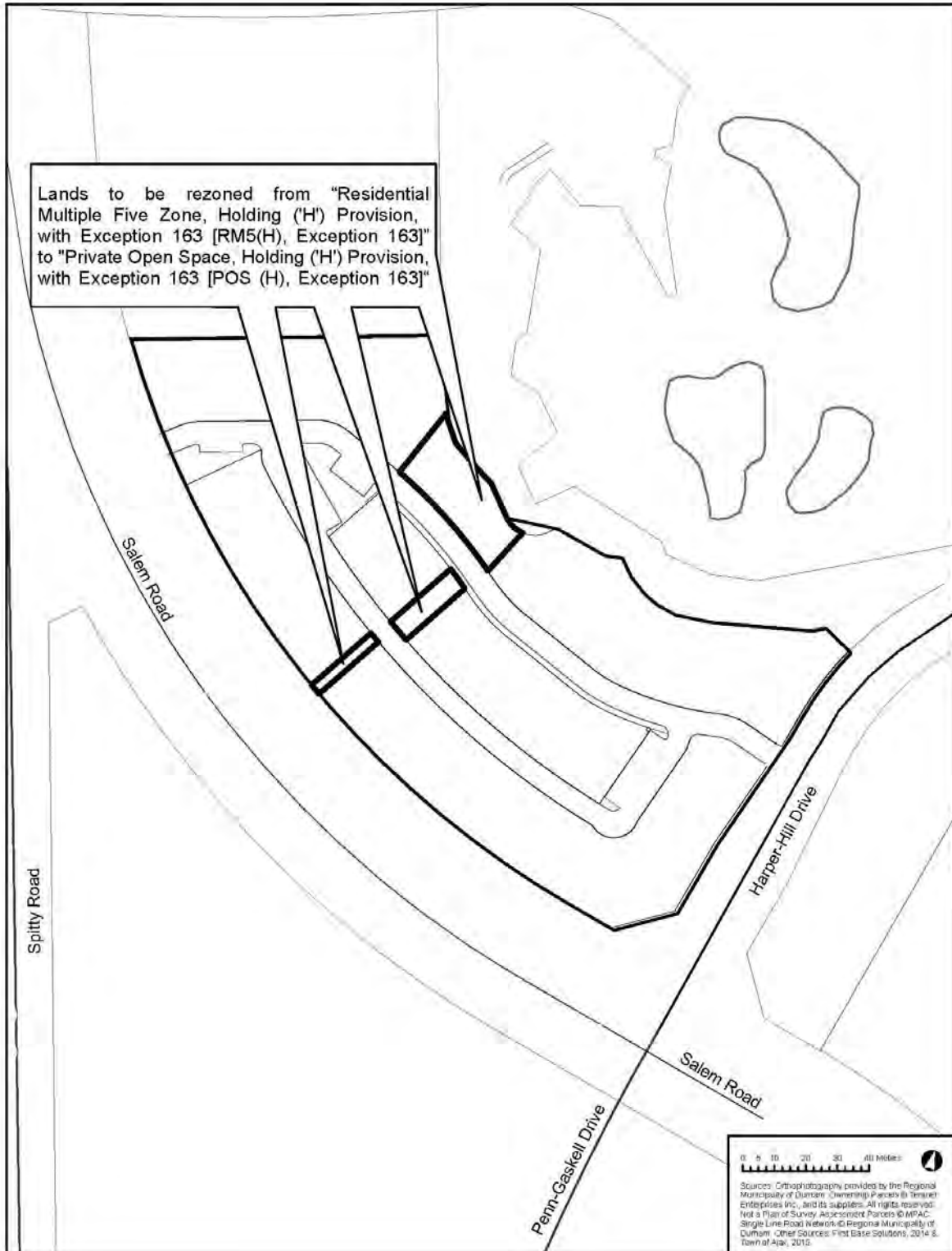
Mayor

D-Clerk

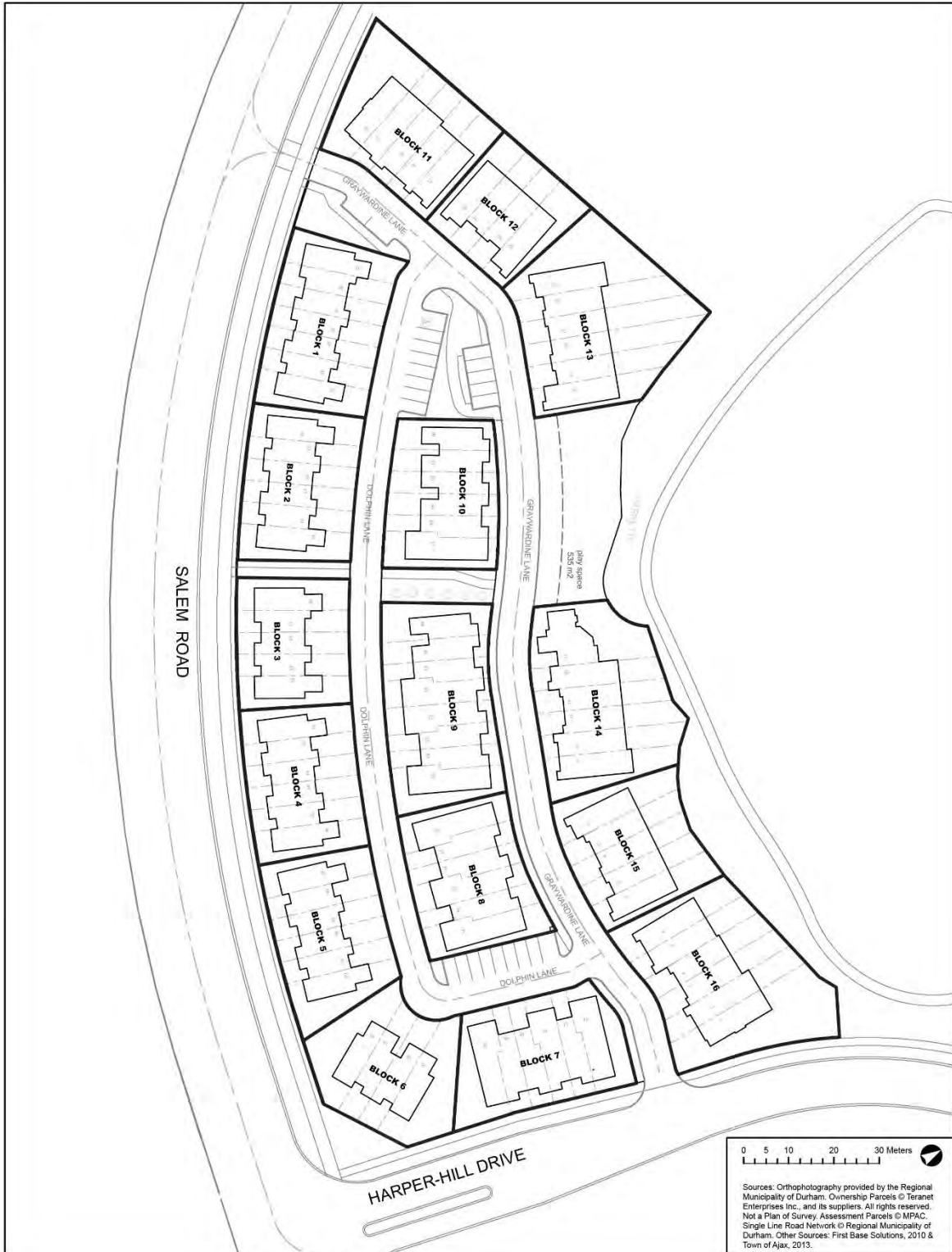
Schedule 'A' to By-Law Number XX-2015



Schedule 'B' to By-Law Number XX-2015



Schedule 'C' to By-Law Number XX-2015



Explanatory Note to By-Law Number XX-2015

The purpose of this by-law is to amend development standards applicable to the subject lands zoned RM5 (H) under Exception 163 of the Town of Ajax Zoning By-law 95-2003, as amended, in order to regulate the development of 92, 3-storey multiple attached dwellings on parcels of tied land (POTL) in a common elements condominium, pursuant Site Plan Application SP4/14, over portions of Block 1 of Draft Plan of Subdivision S-A-2013-01, located at the northeast corner of Salem Road North and Harper-Hill Drive.

The by-law provides development standards for each individual POTL that will be created through the part lot control exemption process and to exempt the development from the requirements of Section 4.9 (Frontage on a Public Road) of the Town of Ajax Zoning By-law 95-2003, as amended.

ATT-2 - Conditions of Draft Approval

Draft Plan of Subdivision S-A-2013-01

Salem Road Subdivision Ltd.

PICKERING CON 3 PT LOT 5 RP 40R26814 PARTS 2 8 AND 9 PT PARTS 1 AND 3

CONDITIONS OF DRAFT APPROVAL

All of the following conditions of draft approval shall be satisfied prior to the final registration of Plan of Subdivision S-A-2013-01 as indicated:

The Regional Municipality of Durham

1. The Owner shall prepare the final plan on the basis of the approved Draft Plan of Subdivision, prepared by Bousfields Inc., identified as Drawing Number 12147-8dp, dated March 5, 2015, which illustrates 1 block for future residential condominium development;
2. The Owner shall name road allowances included in this draft plan to the satisfaction of the Regional Municipality of Durham and the Town of Ajax;
3. The Owner shall submit plans showing the proposed phasing to the Region of Durham and the Town of Ajax for review and approval if this subdivision is to be developed by more than one registration;
4. The Owner shall grant to the Region of Durham any easements required to provide Regional services for this development and these easements shall be in locations and of such widths as determined by the Region of Durham;
5. The Owner shall submit to the Region of Durham, for review and approval, a revised noise report prepared by an acoustic engineer based on projected traffic volumes provided by the Durham Region Planning and Economic Development Department and recommending noise attenuation measures for the draft plan in accordance with the Ministry of the Environment guidelines. The Owner shall agree in the Town of Ajax subdivision agreement to implement the recommended noise control measures. The agreement shall contain a full and complete reference to the revised noise report (i.e. author, title, date and any revisions/addenda thereto) and shall include any required warning clauses identified in the acoustic report. The Owner shall provide the Region of Durham with a copy of the subdivision agreement containing such provisions prior to final approval of the plan;
6. Prior to final approval, the proponent shall engage a qualified professional to carry out, to the satisfaction of the Ministry of Culture, an archaeological assessment of the entire development property, and mitigate, through preservation or resource removal and documentation, adverse impacts to any significant archaeological resources found. No demolition, grading or other soil disturbances shall take place on the subject property prior to the Ministry of Culture confirming that all archaeological resources have been met, including licensing and resource conservation requirements;
7. Prior to final approval, the Owner is required to submit a signed Record of Site Condition (RSC) to the Ministry of Environment (MOE). This RSC must be to the satisfaction of the Region of Durham, including an Acknowledgement of Receipt of the RSC by the MOE;

8. The Owner shall provide for the extension of such sanitary sewer and water supply facilities which are external to, as well as within, the limits of this plan that are required to service this plan. In addition, the Owner shall provide for the extension of sanitary sewer and water supply facilities within the limits of the plan which are required to service other developments external to this subdivision. Such sanitary sewer and water supply facilities are to be designed and constructed according to the standards and requirements of the Region of Durham. All arrangements, financial and otherwise, for said extensions are to be made to the satisfaction of the Region of Durham, and are to be completed prior to final approval of this plan;
9. Prior to entering into a subdivision agreement, the Region of Durham shall be satisfied that adequate water pollution control plant and water supply plant capacities are available to the proposed subdivision;
10. The Owner shall satisfy all requirements, financial and otherwise, of the Region of Durham. This shall include, among other matters, the execution of a subdivision agreement between the Owner and the Region of Durham concerning the provision and installation of sanitary sewers, water supply, roads, services, drainage and other services;

Town of Ajax

11. The Owner shall satisfy all requirements, financial and otherwise, to the Town of Ajax. This shall include, among other matters, the execution of a subdivision agreement between the Owner and the Town of Ajax concerning the provisions and installation of municipal roads, services, drainage, and other local services and matters including the following:

Reports/Studies

- i. The Owner shall provide the following revised technical reports, prior to final approval, to the satisfaction of the Town of Ajax, the Regional Municipality of Durham and the Toronto and Region Conservation Authority:
 - a. Archeological Study;
 - b. Traffic Impact Study;
 - c. Traffic Management Plan;
 - d. Traffic Management Implementation Plan;
 - e. Functional Servicing Report;
 - f. Environmental Impact Study;
 - g. Hydrogeological Report;
 - h. Geotechnical Report;
 - i. Servicing and Stormwater Management Report;
 - j. Noise and Vibration Impact Study; and
 - k. Tree Inventory Report and Tree Preservation Report
- ii. The Owner shall provide, prior to final approval, a detailed Stormwater Management and Servicing Design Report to the satisfaction of the Town of Ajax that must address, but not limited to, the following issues:
 - a. Overland Flow;
 - b. Water Quality and Quantity Control;
 - c. Major and Minor Flow Routes;
 - d. Erosion and Sediment Controls; and

- e. Water Budget Analysis;
- iii. The Owner shall be responsible for any required amendments or further revisions to submitted reports and studies to the satisfaction of the Town of Ajax, the Regional Municipality of Durham and the Toronto and Region Conservation Authority. If a report or study requires any amendments or further revisions the Owner agrees to provide these amendments or revisions prior to the execution of a subdivision agreement with the Town of Ajax;
 - iv. The Owner shall implement all the recommendations in all required reports/studies to the satisfaction of the Town of Ajax, the Regional Municipality of Durham, and the Toronto and Region Conservation Authority;
 - v. The Owner agrees that prior to final approval, the Draft Plan of Subdivision shall be red-lined revised, if necessary, to accommodate the requirements of all the approved reports/studies;
 - vi. The Owner shall retain a qualified professional engineer to verify to the satisfaction of the Town of Ajax that the soil of the site is in compliance with the MOE's "Guidelines for Use at Contaminated Sites in Ontario". A Record of Site Condition acknowledged, and if necessary, audited by MOE may be required to be submitted to the satisfaction of the Town of Ajax prior to registration;

Roads/Sidewalks/Services

- vii. The Owner agrees to employ a Professional Engineer to design all roads, sidewalks, stormwater management facilities and services to the satisfaction of the Town of Ajax;
- viii. The Owner agrees to be fully responsible for the full cost of construction of all roads, sidewalks, walkways, stormwater management facilities and services internal to the subdivision in each respective phase of development to the satisfaction of the Town of Ajax;
- ix. The Owner shall provide the Town of Ajax, detailed engineering drawings in accordance with the Town of Ajax Engineering Design Criteria and Standard Drawings, to the satisfaction of the Town of Ajax, prior to the preparation of a development agreement. In the event that a development agreement is not executed within two (2) calendar years from the date of approval of grading plans for all lots, blocks, right-of-ways, parkette block, landscape blocks, and detailed engineering drawings, the Owner shall be required to resubmit the aforementioned plans and drawings to the Town of Ajax for review and approval prior to the execution of a development agreement;
- x. The Owner agrees at the time of Site Plan Approval for each development Block within the Plan, that the required on-site stormwater management controls and infiltration works be provided as per the approved Functional Servicing Report;
- xi. The Owner agrees to provide a construction management plan, which addresses among other matters, site access, construction traffic, parking for construction trades, material delivery and storage, staging, mud, dust and noise controls, to the satisfaction of the Town of Ajax, prior to the issuance of the first building permit;

- xii. The Owner shall include in all purchase and sale agreements, sales literature and in sales pavilions for homes sold within this development that a trail system may be developed in Open Space Blocks N and Buffer Block W and within the Street A (Harper-Hill Drive) road allowance as indicated on the Beechridge Farms Inc. draft plan of subdivision, and that the exact location of the trail and its construction will be determined prior to new home occupancies;
- xiii. The Owner agrees to install a 3.0 metre asphalt trail along the east side of the Salem Road North road allowance from the south to north limits of the subject lands. The Owner shall enter into a construction agreement with the Town to compensate the Owner for the construction of the 3.0 metre asphalt trail, to the satisfaction of the Town;
- xiv. The Owner shall submit a detailed plan, noting all services/hydrant locations to the Town for approval;
- xv. The Owner agrees to provide fire hydrants in conformity with the requirements of the Ontario Building Code and Region of Durham;
- xvi. The Owner agrees to provide a water supply for firefighting purposes that is accessible and operational at all times;
- xvii. The Owner agrees to maintain, in all phases of development at all times, two remote means of ingress/egress;
- xviii. The Owner agrees to maintain access routes for fire department vehicles to new buildings, construction trailers and material storage areas at all times during construction;
- xix. Prior to final approval, the Owner shall be responsible for preparing a composite utility plan that allows for the safe installation of all utilities, including separation between utilities;
- xx. The Owner agrees to ensure that all streets are constructed in accordance with the composite utility plans previously submitted and approved by all utilities;

Required Payments

- xxi. The Owner shall pay a Stormwater Maintenance Fee, as per the Town of Ajax's "General Fees and Charges By-law", as amended, payable prior to the registration of the subdivision;
- xxii. The Owner shall, prior to registration of the Plan, pay the Final Approval Subdivision Clearance Fee, as per the Town of Ajax's "Planning Act Fees and Charges By-law", as amended;

Reserves/Easements/Conveyances

- xxiii. The Owner shall grant such easement(s) and/or block(s) as may be required for utilities, inspection, drainage, servicing purposes, emergency access or any other purpose to the appropriate authorities or agencies. The easements shall be in locations and widths as deemed appropriate by the applicable authority or agency;
- xxiv. That Owner shall dedicate the road allowances indicated on the Plan as public highway to the Town of Ajax and shall name all streets in accordance with the Town of Ajax policy;
- xxv. The Owner shall identify and provide 0.3 metre reserve along the lot line where the subject lands abuts Salem Road and Harper-Hill Drive;

Design/Landscaping

- xxvi. Prior to offering any model for sale to the public for the subject development, the Builder agrees to comply with the Town's Architectural Control requirements by submitting plans to the Town-approved Control Architect for approval;
- xxvii. The Owner shall provide lot fencing in accordance with the approved plans under SP4/14;

Traffic

- xxviii. Prior to the execution of a subdivision agreement, the Owner shall provide a Traffic Management Plan (TMP), a Traffic Management Implementation Plan (TMIP), and a Utility Coordination Plan (UCP), to the satisfaction of the Town of Ajax;
- xxix. The Owner shall have all traffic control devices and LED street lighting installed to the satisfaction of the Town of Ajax prior to first occupancy. The decision to provide for traffic calming shall be at the sole discretion of the Town of Ajax;

Other

- xxx. The Owner shall submit plans, for review and approval, showing any proposed phasing of development to the Town for review and approval, if this Plan is to be developed by more than one phase;
- xxxi. Prior to the initiation of any works on site the Owner shall satisfy the Town with respect to a construction management plan. The location of the temporary road for construction access will be provided and maintained by the Owner. All parking required for construction and trades shall be provided wholly on-site and not on public streets for the duration of construction;
- xxxii. The Owner shall obtain any necessary permits from the Toronto and Region Conservation Authority prior to the registration of the Plan;
- xxxiii. The Owner agrees to provide a municipal street address for each dwelling unit. The street number must be affixed to the wall of the dwelling that is associated with the front lot line, as defined by the Town of Ajax Zoning By-law 95-2003 or as determined by the Town of Ajax Fire Prevention Services;

- xxxiv. The Owner shall agree in the Town of Ajax subdivision agreement to implement the recommendations of the report entitled “Noise Impact Study Stafford Homes Salem Road and Rossland Road Ajax Regional Municipality of Durham” prepared by J.E. Coulter Associates Limited, dated September 25, 2013, which specifies noise attenuation measures for the development. The measures shall be included in the development agreement and must also contain a full and complete reference to the noise report (i.e. author, title, date and any revisions/addenda) and shall include any required warning clauses identified in the study;
- xxxv. The Owner agrees to include within the condominium documents, including the condominium declaration, the noise warning clause provided in the Noise and Vibration Study prepared by J.E. Coulter Associates Limited, indicating that the adjacent CP Rail facility may be altered or expanded, which expansion or alteration may affect the living environment of residents despite the inclusion of noise attenuating measures;
- xxxvi. The Owner shall provide the following wording in all purchase and sale agreements and within the condominium documents, including the condominium declaration;
- “The development, including all parcels of tied land and lands designated as a common element, are subject to a site plan agreement that has been registered on title. This agreement binds the land owner(s) to build and maintain the site in accordance with the approved plans. Any alterations or deviations require approval from of the Town of Ajax.”
- “Individual access to the Town of Ajax owned lands located east of the subject lands from the rear of adjacent parcels of tied land is prohibited.”; and
- “All air conditioning units shall be located in accordance with the approved plans under SP4/14. For all units where a location has not been identified on the approved plans under SP4/14, air conditioning units shall be located in the rear yard.”;

Toronto and Region Conservation Authority (TRCA)

12. That prior to any grading, development, pre-servicing or site alteration, or registration of this plan or any phase thereof, the applicant shall submit for the review and approval of the TRCA:
- a) A detailed engineering report and plans that describe the storm drainage (quantity and quality) for the proposed development of the subject lands. This report shall include:
- i. plans illustrating how stormwater drainage systems will tie into surrounding drainage systems and storm water management techniques which may be required to control minor or major flows;
 - ii. appropriate Stormwater Management Practices (SWMPs) to be used to treat stormwater, to mitigate the impacts of development on the quality and quantity of ground and surface water resources;
 - iii. proposed methods for controlling or minimizing erosion and siltation on-site and/or in downstream areas during and after construction;

- iv. location and description of all outlets and other facilities which may require a permit pursuant to Ontario Regulation 166/06, (TRCA Regulation of development, interference with wetlands and alterations to shorelines and watercourses);
 - v. proposed locations for the stock-piling of snow.
- b) Grading and Sediment and Erosion Control plans for the subject lands.
13. That all construction activity and restoration landscaping be undertaken in accordance with the findings and recommendations of the Environmental Impact Study and Addendum prepared in support of plan of subdivision application S-A-2005-01.
14. That the applicant obtain all necessary permits required pursuant to Ontario Regulation 166/06.
15. That the owner agree in the subdivision agreement, in wording acceptable to the TRCA:
- a) to carry out, or cause to be carried out, to the satisfaction of the TRCA, the recommendations of the technical report referenced in Condition 12;
 - b) to agree to, and implement, the requirements of the TRCA's conditions in wording acceptable to the TRCA;
 - c) to design and implement on-site erosion and sediment control;
 - d) to maintain all stormwater management and erosion and sedimentation control structures operating and in good repair during the construction period, in a manner satisfactory to the TRCA;
 - e) to obtain all necessary permits pursuant to Ontario Regulation 166/06 from the TRCA;
 - f) to erect a permanent fence, to the satisfaction of the TRCA, for lots and blocks abutting the Environmental Protection areas;
 - g) to provide for planting, restoration and enhancement of all disturbed areas adjacent to or within natural areas to the satisfaction of TRCA staff. And, that monitoring and replanting of these areas be completed for period of a minimum of 2 years, with sufficient funds be secured through this period through a letter of credit or other appropriate measure; and,
 - h) to commit to provide appropriate information to all perspective buyers of lots adjacent to Open Space (valley) blocks through all agreements for purchase and sale, sales information, and community maps to ensure that the land owners are well informed that private use and/or access to the Open Space block shall not be permitted, and reflect the intent of the following:

"The Open Space block adjacent to the subject property is considered to be part of the publically owned valley corridor and will be maintained for environmental protection, and public use purposes. Please note that uses such as private picnics, barbeque or garden areas; and/or the dumping of refuse (e.g. grass/garden clippings, household compostable

goods, garbage etc.) are not permitted on these lands. In addition, access to the valley corridor such as private rear yard gates and/or ladders are prohibited."

16. That the Owner pay the \$15,150 TRCA Review Fee.

Durham District School Board

17. The Owner agrees to post the standard Durham District School Board approved "Notice to Parents" in all sales representation centres.

Canada Post

18. The Owner/Developer agrees to include on all offers of purchase and sale, a statement that advises the prospective purchaser that mail delivery will be from a designated Community Mailbox;
19. The Owner/Developer will be responsible for notifying the purchaser of the exact Community Mailbox locations prior to the closing of any home sale;
20. The Owner/Developer will consult with Canada Post Corporation to determine suitable locations for the placement of Community Mailboxes and to indicate these locations on the appropriate servicing plans;
21. The Owner/Developer will provide the following for each Community Mailbox site and include these requirements on the appropriate servicing plans:
 - i. An appropriately sized sidewalk section (concrete pad) as per municipal standards and to Canada Post specifications to place the Community Mailboxes on;
 - ii. Any required walkway across the boulevard, as per municipal standards; and
 - iii. Any required curb depressions for wheelchair access to Canada Post specifications;
22. The Owner/Developer further agrees to determine and provide a suitable temporary Community Mailbox location(s), which may be utilized by Canada Post until the curbs, sidewalks and final grading have been completed at the permanent Community Mailbox locations;

Communications Service Provider

23. The Owner is hereby advised that prior to commencing any work within the Plan, the Owner must confirm that sufficient wire-line communication/telecommunication infrastructure is currently available within the proposed development to provide communication/telecommunication service to the proposed development. In the event that such infrastructure is not available, the Owner is hereby advised that the Owner may be required to pay for the connection to and/or extension of the existing communication/telecommunication infrastructure. If the Owner elects not to pay for such connection to and/or extension of the existing communication/telecommunication infrastructure, the Owner shall be required to demonstrate to the municipality that sufficient alternative communication/telecommunication facilities are available within the proposed development to enable, at a minimum, the effective delivery of communication/telecommunication services for emergency management services (i.e., 911 Emergency Services);

24. Prior to registration of the Plan of Subdivision, the Developer/Owner will, at its sole cost, grant all necessary easements and maintenance agreements required by those CRTC-licensed telephone companies and broadcasting distribution companies intending to serve the subdivision (collectively, the "Communications Service Providers"). Immediately following registration of the Plan of Subdivision, the Developer/Owner will cause these documents to be registered on title;
25. Prior to registration of the Plan of Subdivision, the Developer/Owner will, with consultation with the applicable utilities and Communications Service Providers, prepare an overall utility distribution plan that shows the locations of all utility infrastructure for the subdivision, as well as the timing and phasing of installation;

Enbridge Gas Distribution Inc.

26. The Owner shall prepare a composite utility plan that allows for the safe installation of all utilities, including required separation between utilities;
27. The Owner shall construct all roads in accordance with composite utility plans previously submitted and approved by all utilities;
28. The Owner shall grade all roads to final elevation prior to the installation of gas lines and provide Enbridge Gas Distribution Inc. with the necessary field survey information for the installation of the gas lines;
29. The Owner agrees that it is understood that the natural gas distribution system will be installed within the proposed road allowance. In the event that this is not possible, easements will be provided at no cost to Enbridge Gas Distribution Inc.;
30. The Owner agrees to provide a 2 metre by 2 metre exclusive use location for a regulator station;
31. The Owner agrees to provide current Town approved road cross-sections showing all Utilities in the configuration proposed for all of the street widths within the development. The gas station must be a minimum of 0.6 metres from the street line.

Veridian Connections

32. The Owner shall make satisfactory arrangements with Veridian Connections for the provision of permanent and/or temporary electrical services to this Plan;

NOTES TO DRAFT APPROVAL

1. As the Owner of the proposed subdivision, it is your responsibility to satisfy all conditions of draft approval in an expeditious manner. The conditions of draft approval will be reviewed periodically and may be amended at any time prior to final approval. The *Planning Act, R.S.O.*

1990 c. P.13 provides that draft approval may be withdrawn at any time prior to final approval.

2. Prior to final approval of this Plan for registration, the Director of Planning and Development Services for the Town of Ajax shall be advised in writing by:
 - The Regional Municipality of Durham, how Conditions 1 to 10 have been satisfied;
 - Town of Ajax, how Condition 11 (inclusive) has been satisfied;
 - Toronto and Region Conservation Authority, how Conditions 12 to 16 (inclusive) have been satisfied;
 - Durham District School Board, how Condition 17 has been satisfied;
 - Canada Post Corporation, how Conditions 18 to 22 (inclusive) have been satisfied; and
 - Communication Service Provider (i.e. Bell Canada Inc., Rogers Cable Communications Inc. etc.), how Conditions 23 to 25 have been satisfied;
 - Enbridge Gas Distribution, how Conditions 26 to 31 have been satisfied;
 - Veridian Connections, how Condition 32 has been satisfied;
3. All Plans of Subdivision must be registered in the Land Titles System within the Regional Municipality of Durham.
4. This Draft Approval shall lapse six (6) years from the date that Draft Approval has been granted if the above-noted conditions have not been fulfilled, or if it has not been extended by the Town.