

The Corporation of the Town of Ajax

## COMMUNITY AFFAIRS AND PLANNING COMMITTEE

Tuesday, February 17, 2015 at 7:00 p.m.

Council Chambers, Town Hall

65 Harwood Avenue South



Confirmed by: *[Signature]*

### AGENDA

**Alternative formats available upon request by contacting:**

[sarah.moore@ajax.ca](mailto:sarah.moore@ajax.ca) or 905-619-2529 ext. 3347

Anything in **blue** denotes an attachment/link. By clicking the links on the agenda page, you can jump directly to that section of the agenda. To manoeuvre back to the agenda page use the **Ctrl + Home** keys simultaneously. **OR** use the “Bookmark” icon to the left of your screen to navigate from one report to the next

**S. Collier, Chair**

**1. Call To Order**

**2. Disclosure of Pecuniary Interest**

**3. Adoption of Minutes**

3.1 **June 16, 2014** ..... 2

**4. Public Meeting**

4.1 **Highway 401 from Brock Road to Courtice Road**  
**Class Environmental Assessment and Preliminary Design**  
**Town of Ajax Considerations**, P. Allore, Director of Planning & Development Services /  
H. Ng, Senior Transportation Planner ..... 6  
~ Presentation by David Fallows, Ministry of Transportation and Patrick Puccini, URS  
Canada Inc.

**5. Presentations**

None

**6. Reports**

None

**7. Adjournment**

**Minutes of the  
Community Affairs & Planning Committee Meeting  
Held in the Council Chambers, Ajax Town Hall,  
At 7:00 p.m. on June 16, 2014**

***Alternative formats available upon request by contacting:  
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Present:	Chair - Regional Councillor	-	S. Collier
	Regional Councillor	-	C. Jordan
	Councillors	-	R. Ashby
		-	J. Dies
		-	P. Brown
	Mayor	-	S. Parish
Regrets:	Councillor	-	M. Crawford

**1. Call to Order**

Chair Collier called the meeting to order at 7:00 p.m. It was noted that Councillor Crawford had sent her regrets.

**2. Disclosure of Pecuniary Interest**

There were no disclosures of pecuniary interest.

**3. Adoption of Minutes**

Moved by: S. Parish  
Seconded by: R. Ashby

That the Minutes of the Community Affairs and Planning Committee Meeting held on June 2, 2014, be adopted.

CARRIED

**4. Public Meetings**

**4.1 Zoning By-law Amendment Z1/14  
Baycliffe Homes Inc.  
Block 189 and Lot 165, 40M-2390  
Northwest corner of Dyerham Drive and Hearson Street**

Alejandro Cifuentes, Planner, delivered a presentation in respect to ZBA Application Z1/14.

Chair Collier declared the meeting to be a public meeting and invited comments. There being none, Chair Collier Closed the public meeting.

Moved by: S. Parish  
Seconded by: C. Jordan

1. That Zoning By-law Amendment Z1/14, submitted by Baycliffe Homes Inc., be approved and that staff be authorized to prepare and forward an implementing Zoning By-law to Council for its consideration at a future Council meeting, as provided within Attachment 1 to this report; and
  2. That the Mayor and Clerk be authorized to enter into an amending subdivision agreement with Baycliffe Homes Inc., to permit four (4) single detached dwelling lots in accordance with Town of Ajax standards including recommendations from the Noise Impact Study – Addendum prepared by YCA Engineering (November, 2013).
- CARRIED

4.2 **Cougs (Workman's) Ltd.**  
**1117 Church Street North**  
**Official Plan Amendment Application OPA14-A1**  
**Zoning By-law Amendment Application Z9/95**  
**Draft Plan of Subdivision Application 18T-95036**  
**Draft Plans of Condominium Application C-A-2014-01**  
**Site Plan Application SP4/12**

Geoff Romanowski, Development Approvals Coordinator, presented in respect to the above noted applications. Opportunity was given for questions and comments. Discussion primarily surrounded the reasons for excluding interior sidewalks in the subdivision design, the differences between a traditional condominium and a 'common element condominium', and concerns related to southbound access from the subdivision during the Church St. bridge reconstruction.

In response to various questions from committee members, the following information was provided by staff and the applicant, Mr. Ian McCullough:

- Streets in the subdivision will be wide enough to accommodate on street parking as well as pedestrians;
- In a 'common element condominium' model, the houses are essentially freehold but residents share common elements such as roads and amenity space;
- Occupancy of the subdivision could occur as early as the summer of 2016. Full occupancy would likely not occur until Spring of 2017 at the earliest;
- The Church St. bridge work will be completed during the summer of 2017;
- The applications before the OMB date back to 2002 but were re-activated in 2010;
- The planned Memory Wall will face westward, and will be visible to persons entering the subdivision from Church St; on street parking will be available in the vicinity of the wall.

Committee members directed staff to ensure that homebuyers are sufficiently notified at the time of purchase of planned construction works and road closures in the area relating to the Rossland Road and Church Street improvements.

Chair Collier declared the meeting to be a public meeting and invited comments. There being none, Chair Collier closed the public meeting.

Moved by: J. Dies  
Seconded by: P. Brown

1. That Official Plan Amendment OPA14-A1, submitted by Cougs (Workman's) Ltd., be approved and that staff be authorized to prepare and forward the amendment to Council at a future meeting, as provided within Attachment 1 to this report;
2. That Draft Plan of Condominium C-A-2014-01, submitted by Cougs (Workman's) Ltd., be endorsed and that staff be authorized to grant draft approval of the draft plan, subject to the proposed draft conditions, as provided within Attachment 2 to this report;
3. That Site Plan SP4/12, submitted by Cougs (Workman's) Ltd., be endorsed and that staff be authorized to grant final site plan approval subject to all drawings, including detailed engineering, landscaping, and related details being finalized, to the satisfaction of the Town of Ajax;
4. That the Ontario Municipal Board be advised that the Town of Ajax supports Zoning By-law Amendment Z9/95, submitted by Cougs (Workman's) Ltd., to amend the Town of Ajax Zoning By-law north of Rossland Road West, east of Church Street North, west of Duffins Creek, and south of 1349 Church Street North in order to implement a residential draft plan of subdivision, as provided within Attachment 3 of this report; and
5. That the Ontario Municipal Board be advised that the Town of Ajax supports Draft Plan of Subdivision 18T-95036, submitted by Cougs (Workman's) Ltd., to permit 140 multiple attached condominium townhouses, a stormwater management pond block, and environmental protection block, a road widening block along the east side of Church Street North, a roundabout block, 2 partial future development blocks, and 2 reserve blocks north of Rossland Road West, east of Church Street North, west of Duffins Creek, and south of 1349 Church Street North, and that the Ontario Municipal Board grant draft plan approval of the draft plan, subject to the proposed draft conditions, as provided within Attachment 4 of this report.

CARRIED

**5. Presentations**

None

**6. Reports**

None

**7. Adjournment (7:30 p.m.)**

Moved by: C. Jordan

Seconded by: P. Brown

That the June 16, 2014 meeting of the Community Affairs and Planning Committee be adjourned.

CARRIED

N. Wellsbury, D. Clerk

## **TOWN OF AJAX REPORT**



**REPORT TO:** Community Affairs and Planning Committee

**SUBMITTED BY:** Paul Allore, MCIP, RPP  
Director of Planning and Development Services

**PREPARED BY:** Hubert Ng, P.Eng.  
Senior Transportation Planner

**SUBJECT:** **Highway 401 from Brock Road to Courtice Road  
Class Environmental Assessment and Preliminary Design  
Town of Ajax Considerations**

**WARDS:** All

**DATE OF MEETING:** February 17, 2015

**REFERENCE:** n/a

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### **RECOMMENDATIONS:**

**That the report to Community Affairs and Planning Committee entitled “Highway 401 from Brock Road to Courtice Road – Class Environmental Assessment and Preliminary Design – Town of Ajax Considerations” dated February 17, 2015, be received for information.**

**That the Ministry of Transportation be advised that the Town of Ajax does not support the introduction of an interchange at Highway 401 and Church Street.**

### **REPORT:**

The purpose of this report is to provide Council with a project update, including the preferred alternative of the Highway 401 from Brock Road to Courtice Road Class Environmental Assessment and Preliminary Design (401EA), together with the potential opportunities and the impact on the Town of Ajax that were discussed between Ministry of Transportation (MTO) and Town of Ajax staff on January 6, 2015.

### **Background:**

The Ministry of Transportation (MTO) initiated the 401EA in June of 2012. The study area is from Brock Road in the City of Pickering to Salem Road in the Town of Ajax and from Brock Street in the Town of Whitby to Courtice Road in Clarington as illustrated in **Figure 1**. The study will establish a preliminary design to widen Highway 401 from 10 lanes to 12 lanes, from Brock Road to the future West Durham Link, and from 6 lanes to 10 lanes, from the future West Durham Link to Courtice Road.

This study includes the integration of the improvements for Highway 401 from Salem Road to Brock Street and the integration of the Highway 407 West Durham Link to Highway 401, as specified in the Highway 407 East Extension project.

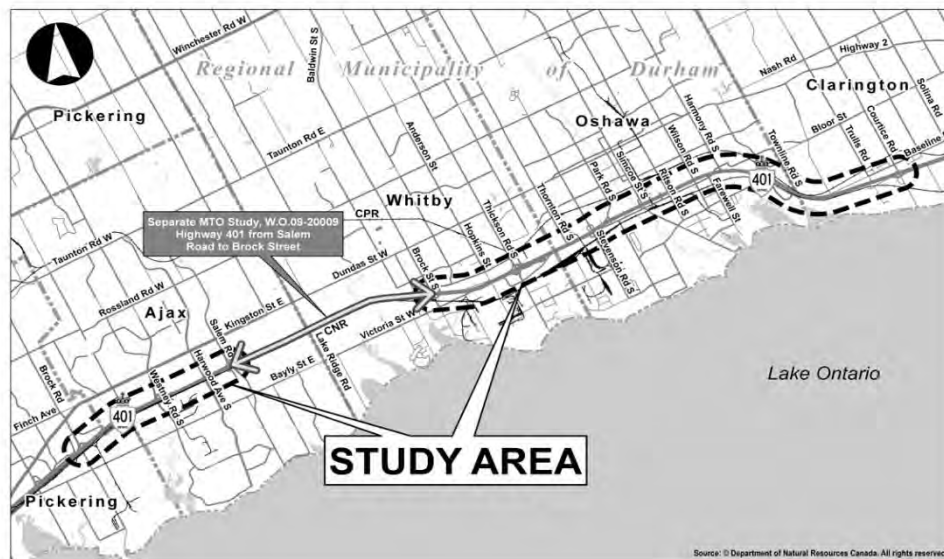


Figure 1: Project Study Area

## Schedule

The 401EA was initiated in June of 2012. The Public Information Centre (PIC) #1 was held on March 20 and 21, 2013 in Oshawa and Ajax, respectively. PIC #2 was also held in Oshawa on November 20, 2014 to present the preferred alternative to the widening of Highway 401.

Town staff provided the MTO with a letter dated November 28, 2014 that outlines the concerns and comments of the Town of Ajax. On January 6, 2015, MTO and Town staff held a meeting to discuss the Town's concerns. These concerns are discussed in detail in the *Impacts on the Town of Ajax* section of this report.

MTO has indicated that the 401EA will be filed in early 2015, once comments received by Town of Ajax staff, other local municipalities, agencies and members of the public have been reviewed and adequately addressed. When 401EA obtains EA clearance, MTO staff will be in a position to proceed to detail design. However, no timetable has been set for detail design, the construction of the rehabilitation or any of the expansion work included in the EA. The recommendations are not currently on the Province's 5-year construction plan. Rather, MTO is focused on developing a rehabilitation schedule that will construct any major bridge structures to its ultimate configuration to accommodate the future widening of Highway 401 in the long-term.

## Summary of Impacts within Ajax

In general, the preferred alternative is to widen Highway 401 equally about the centerline throughout the entire study area. However, a localized shift of the Highway 401 centreline to the north from the Town of Ajax boundary to just east of Salem Road is required in order to avoid significant impacts to the CN Rail line located south of Highway 401. As a result, it is anticipated that 43 residential, 5 non-residential, 2 church and 11 Town properties will be affected. All property owners that were anticipated to be impacted by the recommended plan received a notification letter from MTO prior to the Public Information Centre indicating that their property was anticipated to be impacted. The letter also encouraged the affected property owners to attend the PIC for more information and to discuss the potential impacts to their property with a project representative.

### Residential Properties

During the subsequent detail design phase, representatives from MTO's Property Section will contact affected residents to discuss the impacts to their property and begin negotiations to acquire the required property. Any changes to resident properties will need to conform to the Town of Ajax Zoning By-law. Residents who are directly impacted have the option to sell their homes now to the public or when MTO's property staff begin negotiations to acquire the property during the detail design stage. As a general rule, property owners that are unable to sell their properties at market value for a period of at least 6 months have the option to contact the Ministry and inquire as to whether early purchase of their property may be possible.

### Town Properties

MTO staff have indicated that further discussions on the reconfiguration of Town properties such as Cedar Park, stormwater management facilities and Town roads that are impacted by the recommended plan will be undertaken at the detail design stage once the final property requirements are confirmed. The costs associated with this work will be subject to these future discussions and negotiations between the Town and MTO. A commitment to these further discussions will be included in the Transportation Environmental Study Report. The Town has informed MTO that any reconfigured roadways will need to meet the requirements of the Town's Engineering Design Standards and Drawings. The reconfiguration and construction required will be undertaken by MTO.

Preliminary discussion of the reconfiguration of various Town properties have been initiated and details are presented in this report. Further discussions are needed during the detail design stage. The Town will be financially compensated through discussions with MTO at the detail design stage.

### Stormwater Management

The preferred alternative indicates which water crossing improvements are required. The Stormwater Management Plan for the 401EA will be circulated to the Town of Ajax for comments when it is complete in the winter or spring of 2015.

### Trees

MTO has advised that the tree removal requirements will be confirmed during the detailed design stage, and a commitment for the development of a compensation Landscape Plan during detailed design will be included in the Transportation Environmental Study Report.

Detailed discussions on any impacts include the following:

### **Notion Road to Church Street**

**Figure 2** illustrates the preferred alternative of Highway 401 from Notion Road to Church Street. Highway 401 will be widened on the north in this section. MTO staff have been made aware of a proposed partial interchange at Highway 401 and Church Street identified by a City of Pickering development proposal. MTO have confirmed that no such interchange is included as part of this EA. Town staff have advised the MTO that Ajax is not in favour of a highway interchange at Church Street. A copy of the Community Affairs and Planning Committee Report entitled City of Pickering Zoning By-law Amendment Application A 3/14 1802 & 1902 Bayly Street and 2028 Kellino Road dated June 2, 2014 on the Durham Live application in Pickering has already been forwarded to MTO staff.

The potential for a Highway 401 crossing at Notion Road is being considered in a separate Environmental Assessment underway by the City of Pickering and is not included in the 401EA.



The existing Duffins Creek structure will need to be widened to accommodate the preferred alternative. However, a retaining wall approximately 2 m in height is needed just east of Notion Road on the north side of Highway 401, even without the Highway 401 crossing.



Figure 2: Notion Road to Church Street

### Duffins Creek to Westney Road

**Figure 3** illustrates Highway 401 from Duffins Creek to Westney Road. The Town is concerned with the widening of Highway 401 to the north in this area, as it has a significant impact to the residential properties on the south side of Jacwin Drive. The rear yards of these properties will be significantly reduced. MTO noted that the detailed design stage will determine whether portions or the entire affected properties will need to be acquired by MTO prior to the widening taking place.



Figure 3: Duffins Creek to Westney Road

The existing Westney Road superstructure will be replaced and a new bridge structure is proposed for the westbound Collector lanes over Westney Road to accommodate the widening as illustrated in **Figure 3**. **Figure 4** presents a preliminary draft plan that illustrates the Highway 401 westbound on an off-ramp that has been shifted to the north at Westney Road. This reconfiguration of the westbound on and off-ramps will require Town owned property from the Wright Crescent right-of-way. As previously noted, the configuration and cross section of Wright Crescent will be determined at the detailed design stage but preliminary cross section drawings will be provided by MTO prior to filing of the 401EA.

Further, the Duffins Creek East culvert will need to be extended as illustrated in **Figure 3**. This culvert currently discharges stormwater directly into Duffins Creek without being treated. A copy of the Town of Ajax Stormwater Retrofit Master Plan has been forwarded to MTO for consideration as it recommends a solution to treat the runoff prior to being discharged into Duffins Creek. This recommendation is currently not in the Town's budget.

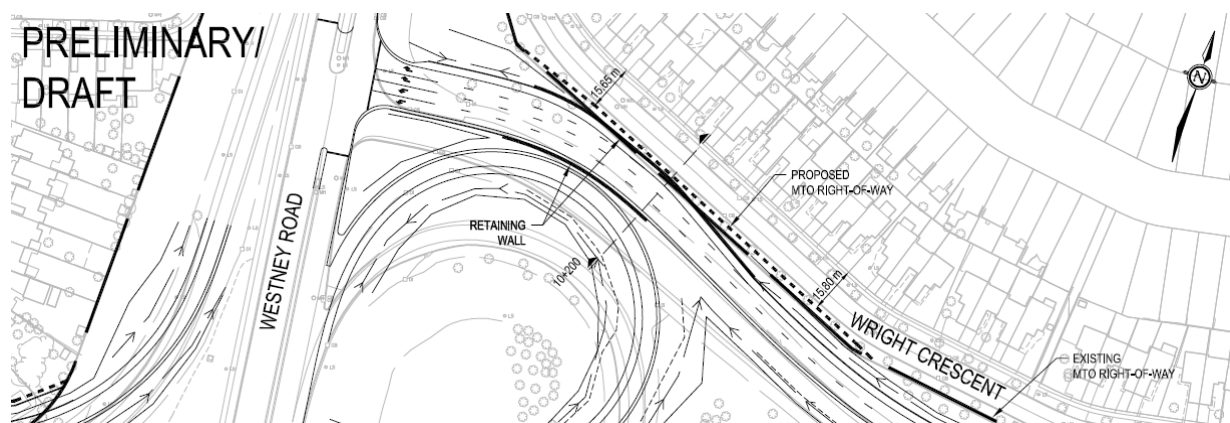


Figure 4: Westney Road

### Reading Street to Cedar Park

The preferred alternative will require a portion of the Reading Street right-of-way. The Reading Street configuration and cross section will need to be designed to meet the Town's Engineering Design Criteria and Standard Drawings during the detailed design stage.

The properties on the south side of Cedar Street will also be affected. The driveways and parking configurations will need to be modified. **Figure 5** illustrates the recommended modifications to these accesses and parking areas at a preliminary design level of detail. The final configuration will be determined during the subsequent detail design stage in consultation with the property owners.



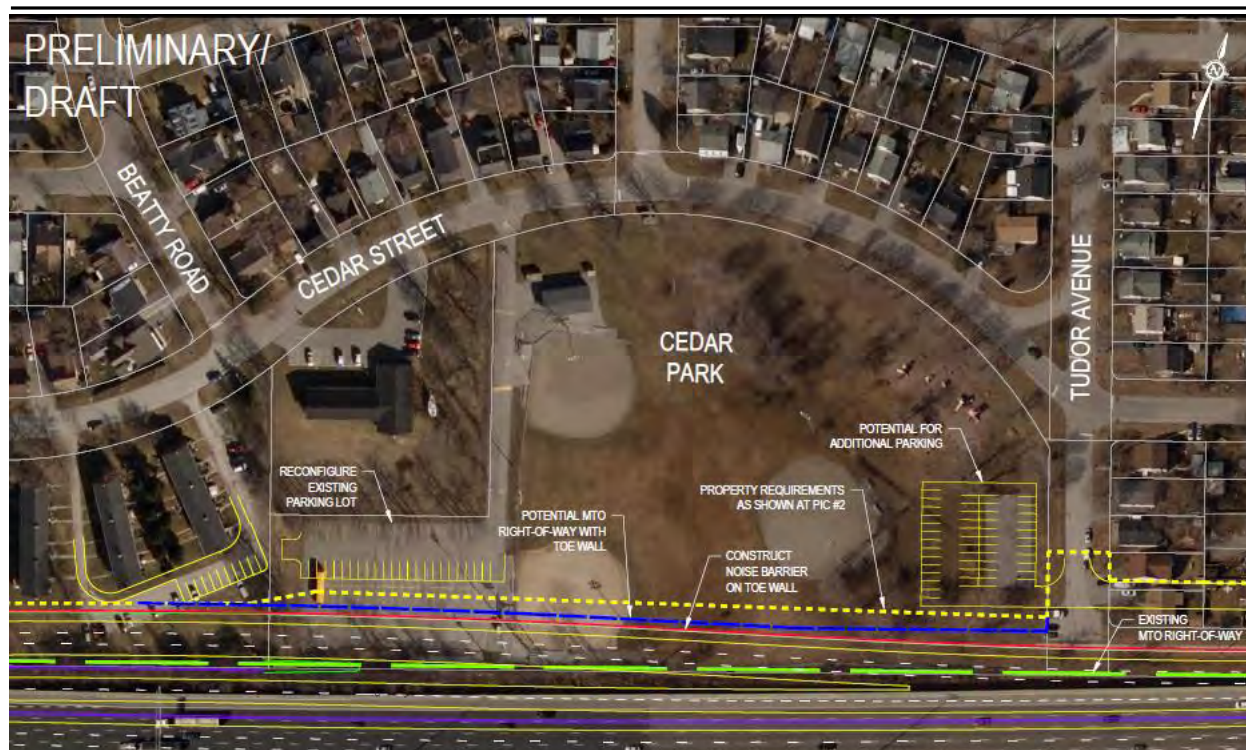


**Figure 5: Reading Street to Cedar Park**

Cedar Park will also be significantly impacted by the preferred alternative as illustrated in **Figure 6**. The southern portion of Cedar Park is required. This will result in the loss of a baseball diamond located in the southwest section of the park. This preliminary draft option illustrates how the park could potentially be reconfigured to compensate for the impacted parking spaces. This and other options will be explored during the detail design stage by MTO in consultation with Town staff. Due to these property impacts, there may be a need to reprogram the space at Cedar Park. In addition, the Town has a number of planned improvements to the park that were identified in the 2015 Capital Budget and the 2016-2019 Long Range Capital Forecast. The capital sheets for Cedar Park are summarized as follows:

- Cedar Park Baseball Improvements – OPS007
  - Reconstruction of the existing batting cages to accommodate young players.
  - Upgrade existing players benches and bleachers in various locations.
  - Fencing repairs at various locations.
  - Budget of \$40,000 in 2015
- Cedar Park Sports Field Lighting Replacement – OPS016
  - Replace floodlights and cross arms.
  - Replace underground wiring and lighting controls.
  - Replace sportsfields lighting poles.
  - Budget of \$10,000 for design fees in 2015
  - Budget of \$150,000 for construction in 2016
- Cedar Park Parking Lot Resurfacing – OPS023
  - Resurfacing of municipal parking lots, including concrete sidewalk and curb repairs as part of the Town's maintenance program to prolong the life of the pavement structure.
  - Budget of \$150,000 in 2019

Operations staff have been notified of the 401EA and how it affects Cedar Park. Operations staff will consider the impacts of the 401EA prior to undertaking any budgeted Cedar Park improvements.



**Figure 6: Cedar Park Reconfiguration Preliminary Draft Option**

### **Tudor Avenue to Harwood Avenue**

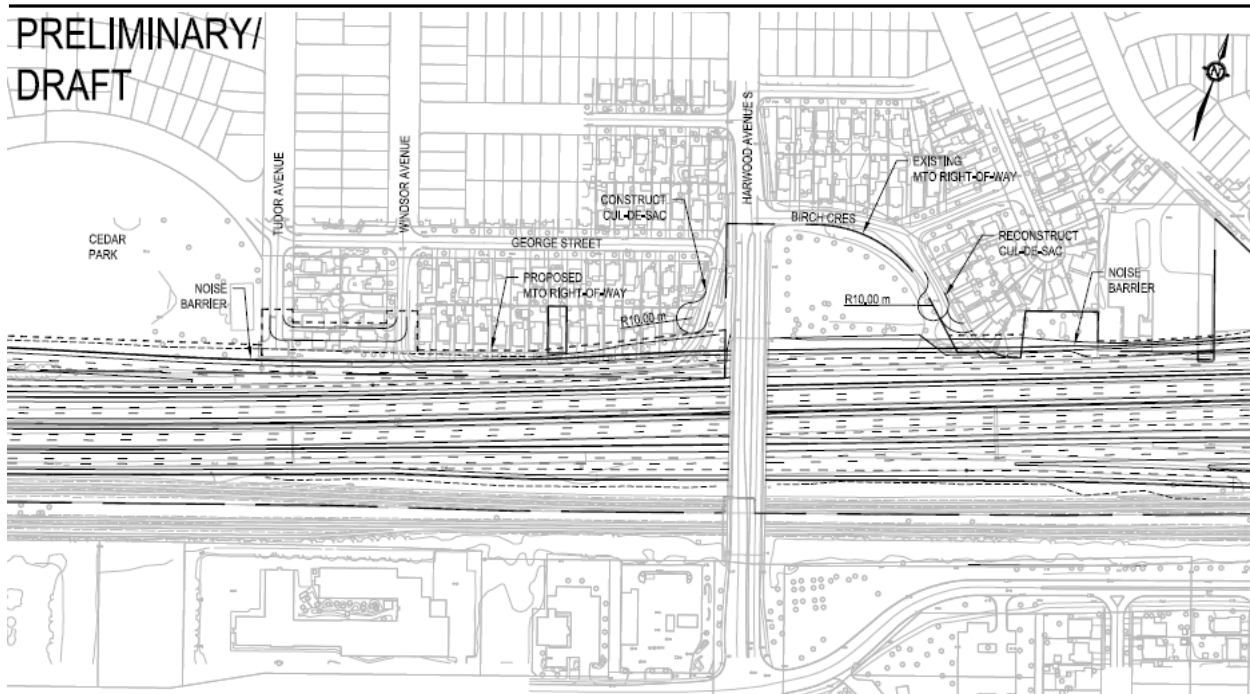
**Figure 7** illustrates a preliminary design option from Tudor Avenue to Harwood Avenue. The preferred alternative will require the southern portion of Tudor Avenue and the adjacent properties. In addition, the Queen Street right-of-way together with the properties fronting onto Queen Street will also be required. In the EA documents, Tudor Avenue is proposed to connect to Windsor Avenue. The southeasterly property of George Street will be required to construct a cul-de-sac at the end of George Street. Town staff have informed MTO that the Town's engineering design standard require a cul-de-sac with a 12m radius as opposed to 10m.

The cul-de-sac on Birch Crescent as well as the most southerly property on Birch Crescent will also need to be acquired by MTO. The cul-de-sac will be shifted north from its current location. Once again, the Town has notified MTO that a 12m radius as well as a secondary access is required for this cul-de-sac relocation.

No changes are required to the Harwood Avenue Structure over Highway 401 as it was already constructed to accommodate the future widening of Highway 401 on the north side.

Further, Town staff have expressed an interest in the Town acquiring surplus MTO property on the northeast quadrant of Harwood Avenue and Highway 401 to facilitate appropriate development opportunities fronting onto Harwood Avenue.





**Figure 7: Tudor Avenue to Harwood Avenue**

#### Harwood Avenue to East of Salem Road

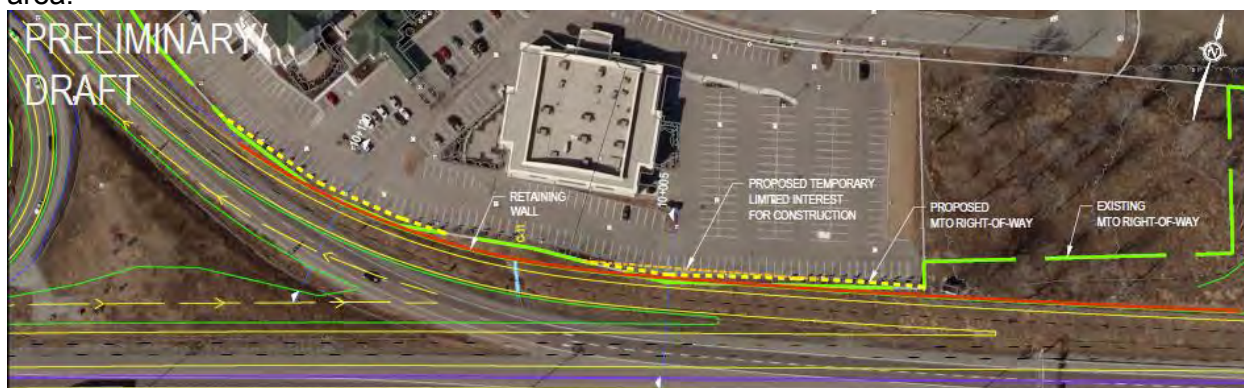
**Figure 8** illustrates the preferred alternative from Harwood Avenue to the east study limits. The Salem Road bridge structure will be widened on both the north and south sides of Highway 401 to accommodate the widening. The existing Highway 401 westbound on and off-ramps at Salem Road will also need to be shifted to the north as illustrated in the preferred alternative.

The stormwater management pond that is jointly owned by MTO and the Town located in the northwest quadrant of Highway 401 and Salem Road will be affected by the minor reconfiguration of the Salem Road interchange. As previously noted, MTO will send Town staff the stormwater management report, which will quantify how the stormwater management pond is affected, for comments when available.



**Figure 8: Harwood Avenue to East of Salem Road**

It is anticipated that the property that includes the Hilton Garden Inn and the Ajax Convention Centre will be minimally affected by the preferred alternative. A small section of the parking lot is anticipated to be temporarily required during the construction stage as illustrated in Figure 9, and a small portion of the property will be permanently required but without impacting the parking area.



**Figure 9: Hilton Property**

#### **FINANCIAL IMPLICATIONS:**

The costs associated with the impacts to Town properties will be the subject of future discussions between MTO and Town staff during the detail design stage once the final property requirements are confirmed. A commitment to these future discussions will be included within the Transportation Environmental Study Report.

Further, Operations staff have been notified of the 401EA and the impacts of 401EA will be assessed prior to undertaking any Cedar Park improvements.

#### **COMMUNICATION ISSUES:**

The following is the primary contact for obtaining additional information:

David Fallows, P.Eng.  
Senior Project Engineer  
Ministry of Transportation, Central Region  
4th Floor, Building D, 1201 Wilson Avenue  
Downsview, ON M3M 1J8  
[david.fallows@ontario.ca](mailto:david.fallows@ontario.ca)  
Phone: 416-235-5519

Please refer to the following website for additional information:

Highway 401 Class EA and Preliminary Design (Brock Road to Courtice Road)  
<http://www.highway401brocktocourtice.ca/>

A copy of staff's previous comments to MTO are also provided for reference.

#### **CONCLUSION:**

As MTO moves forward with the widening of Highway 401 in the future, Town staff will work closely with the province to ensure that the Town's interests continue to be considered as part of the process.

**ATTACHMENTS:**

**1) Previous Staff Comments on EA**

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Hubert Ng, P.Eng – Senior Transportation Planner

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Gary Muller, MCIP, RPP – Manager of Planning

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Paul Allore, MCIP, RPP – Director of Planning and Development Services



**Planning &  
Development Services**

Tel. 905-683-4550  
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**TOWN OF AJAX**

65 Harwood Avenue South  
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November 28, 2014

David Fallows, P.Eng  
Senior Project Engineer  
Highway Engineering – Toronto and Durham  
Ministry of Transportation, Central Region  
Building D, 4<sup>th</sup> Floor, 1201 Wilson Ave  
Downsview, ON M3M 1J8

**Re: Highway 401 from Brock Road to Courtice Road  
Class Environmental Assessment and Preliminary Design Study  
Public Information Centre #2  
Town of Ajax Comments**

Dear Mr. Fallows,

Thank you for facilitating the Public Information Centre #2 (PIC2) on Thursday, November 20<sup>th</sup> for the Highway 401 from Brock Road to Courtice Road Class Environmental Assessment and Preliminary Design Study (401EA).

Given that the preferred solution of widening the Highway 401, which has only been recently presented at PIC2, has significant impacts to the Town of Ajax, we strongly object to the 401EA document being finalized before the end of 2014. Sufficient time is needed to ensure that Ajax's concerns are satisfactorily addressed and Council needs to be briefed concerning the impacts to Ajax residents.

Due to the Municipal Election process undertaken this fall, Council did not meet in October and November of 2014. The earliest opportunity to update Council and for you to receive their comments and recommendations is in the evening of January 19<sup>th</sup> or the afternoon of January 22<sup>nd</sup>, 2015.

We request you appear before Council and make a presentation followed with a questions and answers period on this project on one of the dates mentioned above.

The following comments are being provided by staff, however, we reserve the right to add further comments upon briefing Council in January 2015.

**Impact on Residential Properties:**

- Has the MTO contacted all of the affected property owners informing them of the current preferred solution?
- What is the MTO's timeline and phasing plan to acquire the proposed residential properties?
- Will the MTO only acquire residential properties in its entirety or will only the relevant portions of the properties be acquired?



- All residential private properties affected by the proposed limit change shall conform to the Town of Ajax Zoning By-law 95-2003 (<http://www.ajax.ca/en/doingbusinessinajax/zoning.asp>) as well as the Town's Engineering Design Criteria and Standard Drawings (<http://www.ajax.ca/en/doingbusinessinajax/EngineeringDesignCriteriaandStandardDrawings.asp>).

#### Impact on Town Property:

- The Town has recently put in significant efforts in improving the facilities at Cedar Park. The preferred plan indicates that MTO will require the southern portion of Cedar Park. Please provide clarification on how the MTO will reconfigure Cedar Park to maintain its current facilities and function and what mitigation measures would be provided to the Town for the loss of this parkland.
- Any proposed modifications to the Town of Ajax roads must conform to the Town's Engineering Design Criteria and Standard Drawings that can be accessed in the following location:  
<http://www.ajax.ca/en/doingbusinessinajax/EngineeringDesignCriteriaandStandardDrawings.asp>
- The proposed minor reconfiguration of the Highway 401 Westbound Off-Ramp at Westney Road is in close proximity to Wright Crescent. Please provide clarification on how the MTO proposes to keep the off-ramp segregated from Wright Crescent and if any retaining walls or noise abatement measures are anticipated and if it will result in any reconfigurations to Wright Crescent. Also, if other areas require the relocation of noise abatement walls, then the affected property owners should be identified.
- It is difficult to comprehend the location of retaining walls in the preferred plan. Please provide clarified information.
- The proposed plan requires MTO property acquisition from a number of Town properties to facilitate the widening of Highway 401. Please provide any further information that the MTO may have developed at this stage including any proposed cross sections and proposed configurations for the affected roadways. The Town would like to ensure that a minimum pavement width of 8.5m, a minimum R.O.W of 18.5 and that any existing sidewalks be maintained. The affected roadways include the following:
  - Wright Crescent, 18.5m R.O.W.
  - Reading Street, 18.5m R.O.W.
  - Laneways on the south side of Cedar Crescent, 7.0m Laneway and 2.75x6.0 parking spaces
  - Tudor Avenue, 20.0m R.O.W.
  - Windsor Avenue, 20.0m R.O.W.
  - Queen Street, 20.0m R.O.W.
  - Birch Crescent, 20.0m R.O.W. and bulb turnaround; and
  - Angus Drive, 20.0m R.O.W.
- The proposed minor reconfiguration of the Highway 401 Westbound Off-Ramp at Salem Road is in close proximity to the Hilton and Convention Centre sites and may require property acquisition. Please provide clarification on how the MTO proposes to keep the off-ramp segregated from the property and how MTO will ensure that the existing parking supply of the site is maintained.
- How does the MTO propose to remove/obtain the two most southerly residential properties between Tudor Avenue and Windsor Avenue to extend a window street

between the two avenues? The proposed right-of-way shall conform to our minimum standard (AS-201) for a 15m R.O.W. with an 8.5m pavement width.

- If the proposal removes the Queen Street right-of-way then George Street will require a permanent cul-de-sac bulb as per our Town Standard AS-218, located at the east limit of George Street.

Stormwater Management:

- How does the MTO propose to manage stormwater runoff? Water quality control and treatment should be provided for all areas discharging into watercourses through Ajax. Water quantity control should be provided in all areas where specified by TRCA, including Millers Creek, Carruthers Creek and Lynde Creek.
- Have impacts to the floodplains for all watercourse crossings been explored with hydraulic modelling? The existing TRCA HEC RAS models should be updated with all pertinent grading and crossing changes proposed.

Impact on Trees:

- How is the screening of trees and vegetation impacted? The Town will require the replacement of all trees removed. The replacement will be dependent on the caliper of the trees removed.

Moving forward, we request a meeting with MTO and Town Staff to address the above comments prior to your appearance before Council on January 19<sup>th</sup> or 22<sup>nd</sup> of 2015. We strongly oppose to finalizing the 401EA document prior to addressing Ajax's concerns.

Please respond with your comments to the requests outlined above by: December 15, 2015.

Should you require any clarification on the above matters, please contact Hubert Ng, Senior Transportation Planner at: [hubert.ng@ajax.ca](mailto:hubert.ng@ajax.ca) / 905-619-2529 ext. 3209.

Sincerely,



Paul Allore, M.C.I.P., R.P.P.  
Director of Planning & Development Services  
Town of Ajax  
905-619-2529, ext. 3220  
[paul.allore@ajax.ca](mailto:paul.allore@ajax.ca)

cc: Mayor, Council of the Town of Ajax  
Brian Skinner, CAO (Ajax)  
Dave Meredith, Director of Operations and Environmental Services (Ajax)  
Gary Muller, Manager of Planning (Ajax)  
Carol Coleman, Manager of Engineering – Capital Projects (Ajax)  
Kevin Tryon, Manager of Engineering – Development Approvals (Ajax)  
Hubert Ng, Senior Transportation Planner (Ajax)  
Peter Verok, Regional Director (MTO)  
Jason White, Manager of Engineering (MTO)  
Bob Stevenson, Head of Planning (MTO)  
Mike Sit, Head of Toronto and Durham (MTO)